

Menifee Valley Specific Plan

A master-planned community by
Brookfield Properties



City of Menifee, California
December 19, 2023

Brookfield
Properties

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Section 1

Introduction



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1 - Introduction

1.1 Specific Plan Overview

This Menifee Valley Specific Plan (MVSP) serves as a long-range plan to guide the development of a 590.3-acre mixed-use, master-planned community in an emerging area of Menifee, California. The MVSP sets forth a guiding vision to improve the jobs-to-housing balance in northeastern Menifee, to provide the area with needed recreational amenities, and to deliver a complementary mix of land uses that incorporate regional aesthetic characteristics in a contemporary 21st century community.

The MVSP provides for eight land uses. The *Residential* area consists of neighborhoods containing single-family and multi-family homes, potential community farm, and green spaces to provide an open feel. A *School* is allocated at the center of the Residential area for the development of a K-5 school site, or residential land uses. Active and passive *Recreation* and *Conservation* uses include a public sports park and the preservation of Granite Hill complete with permanently-preserved open space and walking trails. A greenbelt with a meandering multi-purpose trail connects the MVSP with the community of Heritage Lake to the south via a future planned pedestrian bridge. Next, a *Public Facility - Civic Node* is reserved in the southwest corner of the site for a fire station, a passenger rail stop, and/or other civic use. To attract employment-generating uses, offer commercial/retail opportunities, and spur economic investment in Menifee, *Business Park*, *Commercial Business Park*, and *Commercial* areas are positioned along Highway 74 and Menifee Road. The combination of these uses establishes a balanced, self-contained, lively and prosperous community where people can live, invest, work, shop, and spend leisure time within a walkable radius.



Residential



Recreation



Public Facility - Civic Node



Commercial



Business Park



Commercial & Commercial Business Park

When determining the optimal mix and arrangement of land uses, the property's location was the inspiration for integrating multiple land uses into one master-planned community.

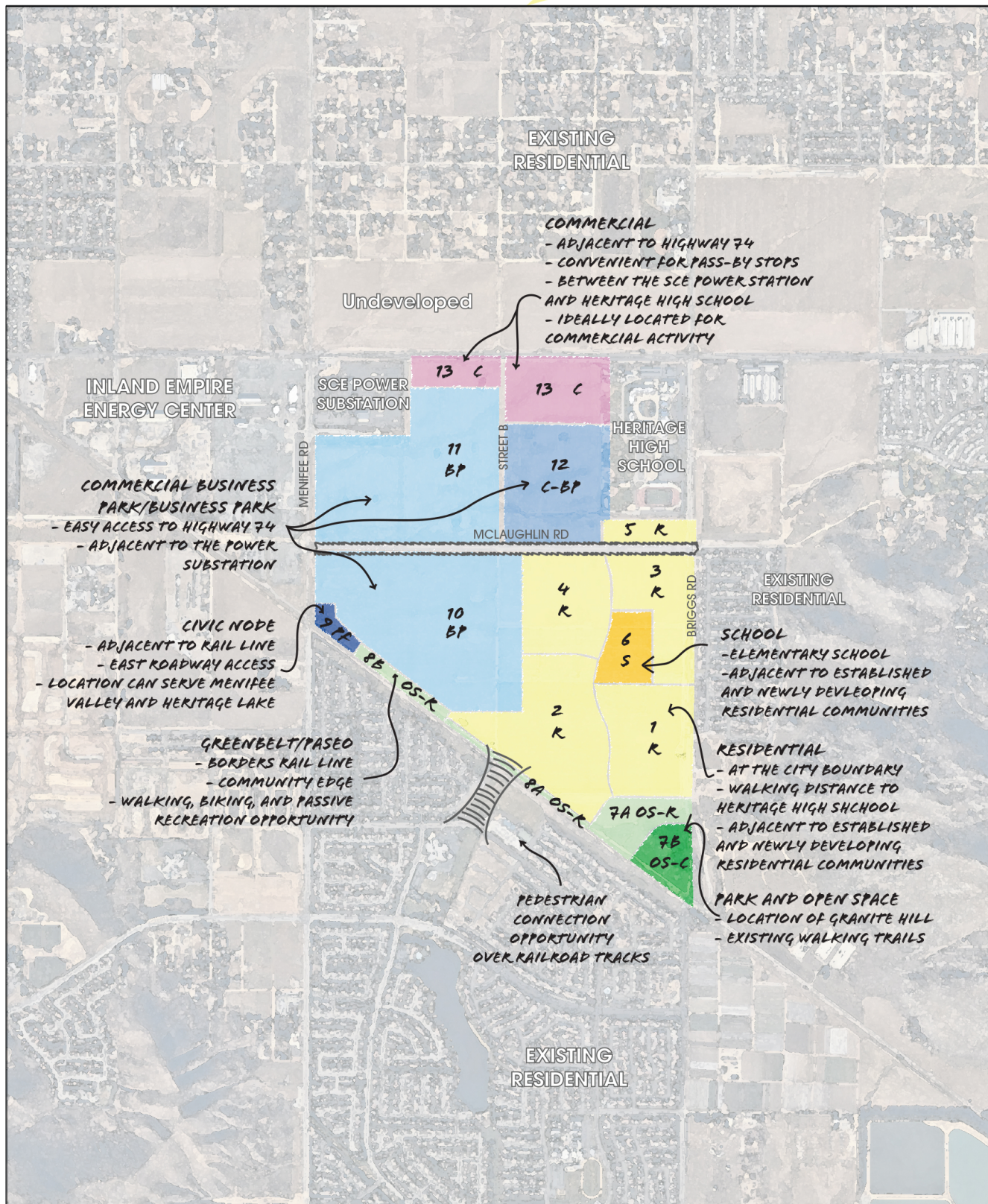


Looking northeast across Meniffee Valley.

Refer to Figure 1-1, *Surrounding Land Uses and Specific Plan Vision*. The eastern portion of the site is ideal for a residential community, complete with single-family and multi-family homes, greenbelts, parks, and allowances for an elementary school site and community farm, drawing on the rural and agrarian history of the area. This Specific Plan encourages diversification among lot sizes and housing types in the Residential planning areas to provide a range of home sizes and configurations to welcome a variety of household types. To the northeast is Heritage High School, a public high school of the Perris Union High School District and within a short walking distance to the MVSP's residential areas. To the south, across the Riverside County Transportation Commission (RCTC) rail line, is the residential community of Heritage Lake and to the east, across Briggs Road, are several new residential subdivisions in unincorporated Riverside County.

To provide a balance of residential and non-residential uses, the northern and western portions of the site are ideal locations for Commercial and Business Park development. Commercial and Business Park opportunities along Highway 74 and Meniffee Road serve future residents of Meniffee Valley, as well as residents of the City of Meniffee and the region, passers-by, and job seekers. Concentrating these uses in the northern and western portions of the MVSP takes advantage of designated truck routes for commerce vehicles and provides a compatible transition use next to the Southern California Edison's (SCE's) Valley Substation. Further, these employment-generating uses create positive economic benefits to the City, including new net revenues to the General Fund. The flexibility of the MVSP in allowing for a variety of Commercial and Business Park uses is a critical component of the plan as it encourages ingenuity in future development/operational proposals.

In the southwestern corner of the MVSP area is a Public Facility - Civic Node (Planning Area 9), placed here for use by the City of Meniffee as a fire station, transit stop, and/or other civic use. The location is positioned next to a RCTC rail line that may serve uses in the MSVP and nearby areas.



Above-ground utilities from SCE's Valley Substation, and regional underground water, sewer, and storm drain lines are located along McLaughlin Road, and are maintained in their existing alignments within the MVSP area.

The MVSP also features a comprehensive recreation and green space network that preserves the hillsides that comprise Granite Hill in the southeastern corner of the site within Planning Area 7B, provides a public sports park, and treats greenbelts, linear paseos, streetside green spaces as "outdoor rooms" with details as important as individual homes and businesses. Combined with a pedestrian bridge for access across the railroad tracks to the south, the parks and schools of the MVSP and Heritage Lake community are connected, enhancing walking, biking, and overall mobility between the two communities and larger region.

As summarized in Table 1A, *Land Use Summary*, the MVSP's land uses include 186.8 acres of Residential uses (with up to an allowable 1,718 dwelling units), 15.5 acres for a School site, 44.5 acres of Open Space - Recreational and Open Space - Conservation uses (including but not limited to a sports park, greenbelts, and a passive open space park on and around Granite Hill), 215.5 acres of Business Park uses, 47.7 acres of Commercial-Business Park uses, 42.6 acres of Commercial uses, 5.3 acres for a Public Facility - Civic Node site, and approximately 32.4 acres for Backbone Public Roadways.

Table 1A - Land Use Summary

Land Use Designation	Acres	Maximum Buildout	Description
Residential (R)	186.8	1,551 units ¹	Attached and Detached Dwellings
School (S)	15.5	167 units ¹	School, Attached and Detached Dwellings
Recreation (OS-R)	29.8	--	Parks, Greenbelts, Open Space
Conservation (OS-C)	14.7	--	Hillside Conservation, Existing Trails
Public Facility - Civic Node (PF)	5.3	120,000 s.f.	Fire Station, Transit Stop, Other Civic Uses
Business Park (BP)	215.5	4,360,000 s.f.	Jobs, Commerce
Commercial-Business Park (C-BP)	47.7	1,150,000 s.f.	Commercial, Retail, Jobs, Commerce
Commercial (C)	42.6	560,000 s.f.	Commercial, Retail, Jobs
Backbone Public Roadways within the MVSP (listed to the right)	32.4	--	Highway 74, Menifee Road, Malaga Road, McLaughlin Road, Briggs Road, McKinley, Biscayne
Total Acreage	590.3	1,718 units ¹	
Note: 1. Planning Area 6 is expected to be developed as a school site. If not desired for a school site by the Romoland School District, Planning Area 6 will be developed with residential uses as described herein. Units allocated to Planning Area 6 are included in maximum buildout of residential units.			

This Specific Plan guides development of the MVSP area by establishing land use designations, infrastructure plans, development standards, and design guidelines addressing building placement, architectural style, landscape, and other project elements. The design elements create visual interest, thematic coherence, and compatibility through the consistent use of complementary exterior building materials, colors, and façade treatments. A landscape program ties together the hardscape and softscape elements to create a cohesive visual character amongst the varied land uses. Implementing development projects within the boundaries of the MVSP are required to demonstrate substantial conformity with the vision and guidelines contained in this Specific Plan document as detailed in Section 7, *Administration and Implementation*.

1.2 Location

As shown on Figure 1-2, *Vicinity Map*, the MVSP is located in the northeastern portion of the City of Menifee, in Riverside County, California, which is part of a larger area of southern California known as the Inland Empire. Regional access to the MVSP area is provided from the State Highway System via Interstate 215 (I-215) located approximately two miles to the west and via State Route 74 (SR-74; locally known as “Highway 74”) which forms the northern boundary of the MVSP. More specifically, the plan area is bound by SR-74 to the north, Menifee Road to the west, railroad tracks and Matthews Road (a dirt road at the time this MVSP was prepared) to the south, and Briggs Road to the east. Briggs Road also serves as the eastern boundary of the City of Menifee and abuts unincorporated Riverside County.

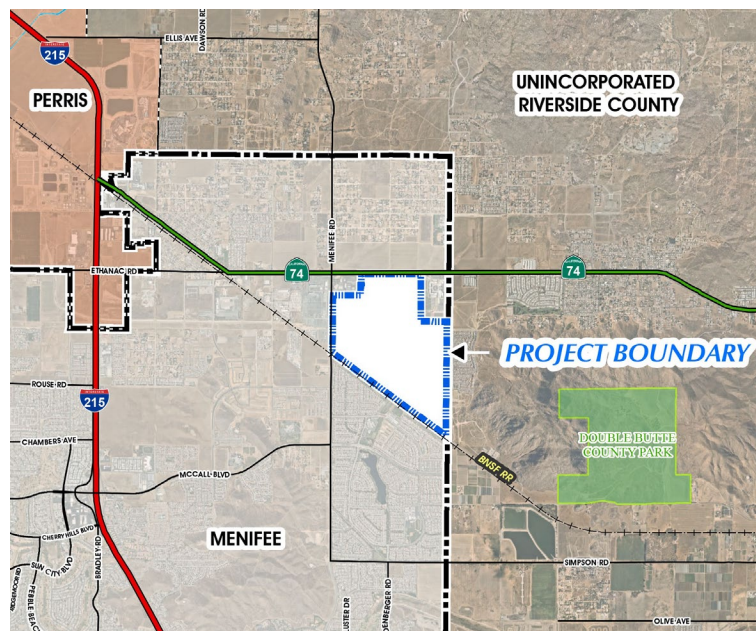


Figure 1-2, Vicinity Map

1.3 Existing Site Characteristics and Topography

At the time this Specific Plan was prepared, the site was utilized for agriculture and flat to gently sloping other than the presence of Granite Hill in the southeast corner of the property. As shown on Figure 1-3, *USGS Topographic Map*, the southeastern corner of the site features a sizable granite rock formation called Granite Hill and captures the site's highest elevation. The elevations of the site average approximately 1,490 above mean sea level (amsl) across the site, with the highest point being located in the southeast corner at approximately 1,628 amsl. Figure 1-4, *Aerial Photograph 2023*, shows the existing condition of the property at the time this MVSP was prepared.

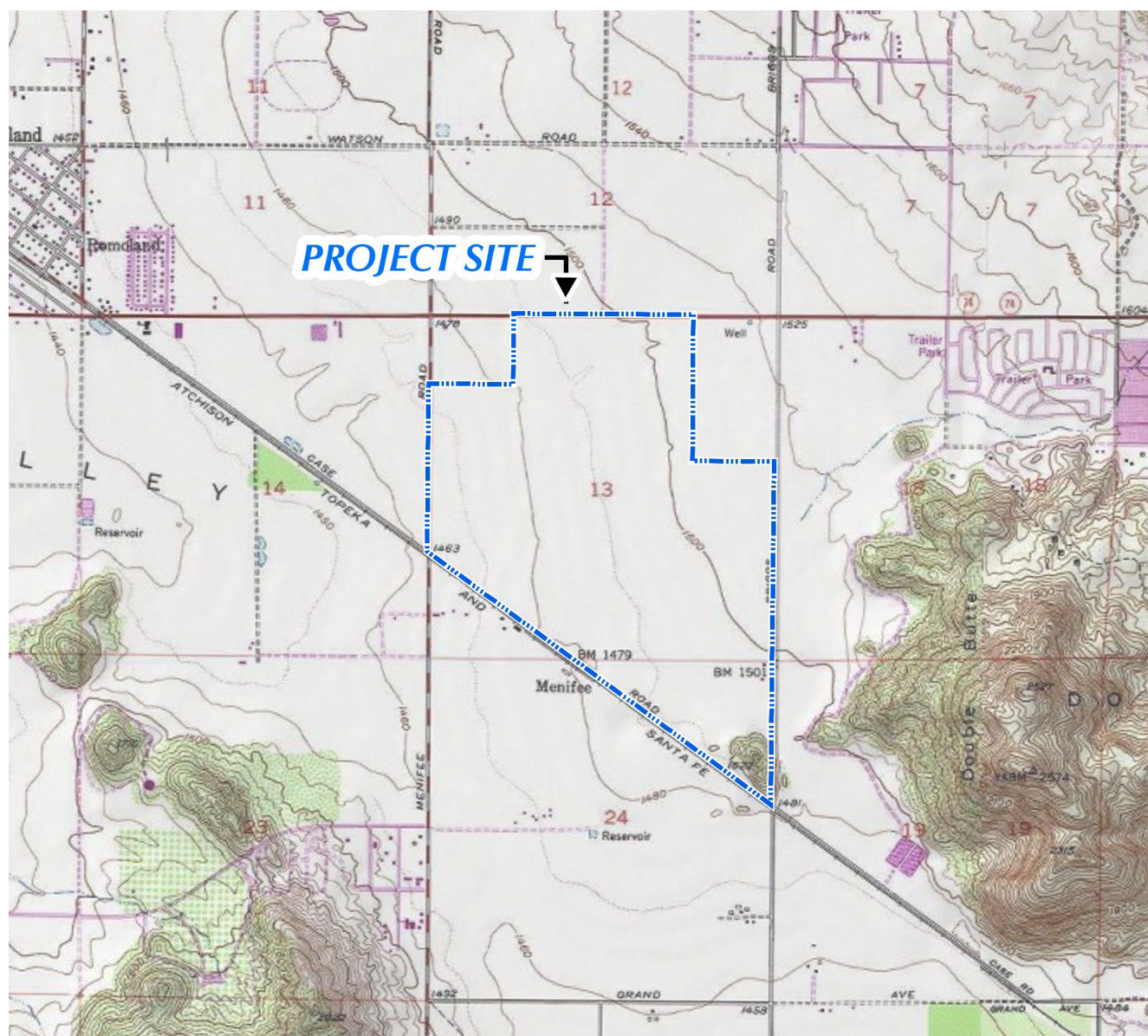


Figure 1-3, USGS Topographic Map





Source(s): ESRI, RCTLMA (2021), Nearmap Imagery (2023)

Figure 1-4



Aerial Photograph, 2023

1.4 Entitlement History

At the time this document was prepared, the 590.3-acre MVSP area was located within the approved Menifee Valley Ranch Specific Plan No. 301 (SP301) originally adopted by the Riverside County Board of Supervisors on April 29, 1997. Since its original approval and incorporation into the City of Menifee, a number of amendments to SP301 have occurred and a majority of the southern portion of SP301 (south of Matthews Road) was developed per the original SP301 and its subsequent amendments. This area south of the MVSP is known as the community of Heritage Lake. Additionally, a RCTC rail line that bisects SP301 was developed to parallel the alignment of Matthews Road, an existing dirt road. At this time this MVSP was prepared, the rail line was inactive but planned to be active in the future. The RCTC rail line acts as a distinct barrier within SP301, separating the developed Heritage Lake portion of SP301 from the MVSP area. The MVSP and concurrent amendment to SP301 will remove the MVSP area from SP301 so that the MVSP is a separate and distinct Specific Plan area.

1.5 Specific Plan Objectives

During preparation of the MVSP, many factors were considered to ensure a thoughtfully designed community that complements the physical and cultural fabric of northeastern Menifee. Using the City of Menifee's General Plan 2030 and the City's Design Guidelines (April 2020) as guidance, following are the overall objectives of the MVSP:

- Implement the City of Menifee's General Plan, which envisions that the geographic area governed by the MVSP will be developed into a high-quality master planned community that demonstrates consistency with the City's General Plan policies.
- Plan for the development of a contemporary mixed-use community that internally balances housing needs and community amenities with job-producing businesses that are economically viable in a 21st century economy.
- Locate businesses that rely on transportation efficiency along Menifee Road and Highway 74, which are established truck routes.
- Ensure that Business Park, Commercial-Business Park, and Commercial areas are designed as places where businesses can prosper, attract economic investment to the City of Menifee, and provide goods, services, and job opportunities to the surrounding community and region.
- Concentrate residential uses in the eastern portion of the property and provide opportunities in the Residential areas for supportive uses that are important to households such as a K-5 school, green spaces, recreational amenities, and agri-commercial uses such as a community farm.

- Physically separate Residential, Commercial Business Park, and Business Park areas through traditional and creative means such that the uses are complementary and supportive while limiting real and perceived conflicts associated with the adjacency of these uses.
- Create gathering spaces and encourage outdoor movement in the form of parks, paseos, streetside green spaces, and outdoor employee amenity areas.
- Position a public facility/civic node in a convenient location that provides opportunity for a new fire station, a new rail corridor transit stop, and other public or quasi-public uses.
- Preserve Granite Hill in permanent open space, while allowing trails and other non-invasive activities that will protect the tangible and intangible assets of the landform. No development or fuel modification is permitted within Planning Area 7B, except as necessary for the construction of Briggs Road.
- Provide a comprehensive circulation network with integrated mobility options by introducing traffic calming features in the Residential areas, by providing pedestrian and bicycle paths and amenities throughout the community, and by providing a non-vehicular bridge connection to the Heritage Lake community to the south.
- Identify and implement infrastructure improvements to provide adequate and reliable water, reclaimed water, sewer, and storm drain service for the community.
- Create a cohesive architectural and landscape theme that ties the various components of the community together to appear as a unified, defined and recognizable place.

1.6 Specific Plan Organization

This Specific Plan is a regulatory document. It establishes the land use designations, building intensities, development standards (zoning), design guidelines, and implementation procedures for buildout of the 590.3-acre Menifee Valley Specific Plan area. In addition, infrastructure improvements such as roads, water, wastewater, and storm water drainage systems are addressed by this Specific Plan to ensure their proper sizing and timely installation. This MVSP is organized into the following seven sections:

Section 1: Introduction. Provides planning context, background, location, and a brief overview of the land use plan. This section also summarizes the purpose, intent, authority, and scope of the Specific Plan.

Section 2: Land Use Plan. Establishes the land use plan for the MVSP and provides detail on each land use type and the Specific Plan's 13 planning areas. A "planning area" is a defined geographic area in which this Plan establishes development standards, allowable uses, and design guidelines are uniformly applied.

Section 3: Mobility and Infrastructure Plans. Describes the buildout of the Specific Plan and its supporting infrastructure systems, including but not limited to the vehicular and non-vehicular circulation plan, domestic and reclaimed water plans, sewer plan, drainage plan, open space and recreation plan, and grading plan.

Section 4: Development Standards. Establishes the standards (zoning) by which development is implemented in the Specific Plan area. Standards are established for each land use type. Standards tailored for the MVSP supersede and supplement standards contained in the City's zoning ordinance to provide context-appropriate standards that take the Specific Plan's location, surrounding land uses, and vision for buildout into account.

Section 5: Landscape Guidelines. Contains guidelines for landscape throughout the MVSP area, including plant material selection, placement, and design. The guidelines also address the design of common areas such as streetscapes, entry monuments, parks, greenbelts, streetside green spaces, and the features that serve buffering and separation purposes between the residential and non-residential uses identified in the Specific Plan. Guidelines for freestanding walls, fencing, lighting and signage also are included.

Section 6: Architectural Guidelines. Contains guidelines for the vertical architectural design of all structures in the Specific Plan area. The guidelines address overall architectural character as well as detailed architectural elements to ensure high-quality design.

Section 7: Administration and Implementation. Describes the administrative procedures required for implementation of the MVSP, including phasing, financing mechanisms, and responsibilities for perpetual maintenance of common areas. This section also specifies procedures for the review and approval of implementing development plans that substantially conform to the MVSP and summarizes the criteria by which the City may require amendments to this Specific Plan.

1.7 Regulatory Setting

1.7.1 Purpose of the Specific Plan

Authorized by the California Government Code § 65450 et seq., a Specific Plan is a planning tool that is used for the systematic implementation of the General Plan for all or part of the area covered by the General Plan. It effectively establishes a link between implementing policies of the General Plan and the individual development proposals in a defined area. As such, this document provides the City of Menifee a mechanism to ensure the efficient and orderly development of the MVSP area in ways that will also be consistent with the City's General Plan 2030.

All future implementing actions (development plans, tract maps, plot plans, conditional use permits, site plans, and other entitlements) for property located within the boundaries of this Specific Plan are required to be consistent with the standards set forth in this document and with all other applicable City of Menifee regulations, as discussed in Section 7, *Administration and Implementation*. This

Specific Plan provides text and exhibits that describe the various land uses, amenities, and infrastructure improvements envisioned to occur in the Specific Plan area.

1.7.2 Severability

This Specific Plan document enables the City of Menifee to facilitate the processing and approval of implementing permits and approvals necessary for development in the MVSP. If any regulation, condition, program, clause, phrase, or portion of this Specific Plan is held invalid or unenforceable, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such portions or provisions shall not affect the validity and enforceability of the remaining provisions contained in this Specific Plan.

1.7.3 Authority and Scope of this Specific Plan

California Government Code §65450 through §65454 establish the authority to adopt Specific Plans, identify the required content of a Specific Plan, and mandate consistency with the applicable General Plan. According to § 65450, a Specific Plan is required to include text and a diagram or diagrams which specify all of the following details:

- The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
- The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
- Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
- A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out the items listed above.
- A statement of the Specific Plan's relationship to the General Plan.

California state law also provides for the inclusion of any other subject that, in the judgement of the local planning agency, is deemed necessary or desirable to implement the General Plan, such as architectural or landscape design guidelines.

This MVSP includes all of the required elements listed above, as well as architectural and landscape guidelines. This Specific Plan also provides the essential link between the MVSP and the applicable policies of the City of Menifee General Plan 2030. All implementing development plans are required

to substantially conform to the standards and guidelines set forth in this document, as well as all other applicable City regulations.

1.7.4 Relationship to the City's Zoning Code

This Specific Plan provides development standards in Section 4 intended to serve as zoning for the MVSP area. Items on which this Specific Plan is silent, applicable provisions of the City of Menifee Zoning Code apply. Where a conflict exists between the standards contained herein and those found in the City of Menifee Zoning Code, the standards in this document apply. Refer to Section 7, *Administration and Implementation*, for more detail regarding interpretations, authority, and administration of the Specific Plan.

1.7.5 Companion Actions

Along with the consideration of this Specific Plan for approval, the City of Menifee concurrently considered the following actions,

Environmental Impact Report Certification. An Environmental Impact Report (EIR) prepared as the California Environmental Quality Act (CEQA) compliance document for the entitlement (approval) of this Specific Plan. The EIR discusses consistency between this Specific Plan and the City of Menifee General Plan 2030, and provides mitigation measures to avoid or reduce the environmental effects resulting from the Specific Plan's implementation. Please refer to the EIR and its accompanying mitigation monitoring and reporting program (MMRP) for more information.

Amendment to SP301. In order to approve the MVSP, the property needed to be removed ("detached") from the Menifee Valley Ranch Specific Plan (SP301), which was originally adopted by the Riverside County Board of Supervisors in April 1997, before the City of Menifee incorporated as a city. A concurrent amendment to SP301 detaches the MVSP area from SP301, allowing the MVSP to be a separate and distinct Specific Plan area.

Development Agreement. A Development Agreement executed between the City of Menifee and the Specific Plan sponsor. The Agreement refers to this Specific Plan for the allowable land uses in the Specific Plan area and outlines other terms and conditions of approval associated with the Specific Plan's approval and implementation. To strengthen the public planning process and reduce the economic risk of development, California Government Code sections 65864 et seq. authorizes government agencies such as the City of Menifee to enter into agreements with parties having a legal or equitable interest in real property regarding the development of the property.

Change of Zone. A Change of Zone is required to revise the City Zoning Map to include this Menifee Valley Specific Plan (Specific Plan No. PLN 21-0217) zone. Additionally, the Change of Zone is required to remove the MVSP property from the SP301 zoning ordinance text.

Tentative Tract Map. A Tentative Tract Map includes a subdivision to establish the boundaries and dimensions of streets and the proposed mass grading for the MVSP area (Specific Plan No. PLN 21-0217). Following map recordation, the final map will become the legal document that identifies the lots and backbone infrastructure to allow for future subdivision maps to be filed.

Section 2

Land Use Plan



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2 - Land Use Plan

2.1 Development Concept

The vision for this Specific Plan is a new master-planned community in the City of Menifee that offers a self-contained balance of uses. Establishing a balance of land uses within a single community promotes vitality, long-term economic stability, and offers employment choices for residents in a very short commute. A balanced community also offers housing types that are attractive to diverse households and provides amenities that promote happy and active lives. The Menifee Valley Specific Plan (MVSP) is built on the principle of balance. Table 2-A, *Detailed Land Use Summary*, provides statistical information for each planning area and Figure 2-1, *Conceptual Land Use Plan*, depicts the planned land uses and acreages.

An illustrative site plan showing one of many potential buildout concepts for the MVSP is presented in Figure 2-2, *Conceptual Illustrative Site Plan*. Figure 2-2 is conceptual and is provided for illustrative purposes only to assist in visualizing how the community could be developed. The ultimate development and site plan configurations will vary from what is shown in Figure 2-2 and will be determined at the time that implementing development projects are brought forward for review and approval by the City of Menifee in accordance with this Specific Plan. A description of the Specific Plan's land uses and vision is provided below.

In the eastern portion of the community, the MVSP offers residential neighborhoods with an array of housing types and amenities. Amenities include but are not limited to: a private recreation center, greenbelts with multi-purpose trails and paths, a dog park, and an agri-commercial area intended to promote farm-to-table activities. The preservation of Granite Hill as a passive park, the inclusion of a large public sports park, and space for an elementary school site are included to meet year-round public recreation and educational needs. The residential area is envisioned to encourage walking and bicycling to destinations close to home through pedestrian-friendly street patterns that offer residents a pleasant walking/riding environment. A pedestrian bridge is planned over the railroad tracks that separate Menifee Valley from Heritage Lake, conveniently linking the two communities together and overcoming the physical barrier introduced by the rail line.

The northern and western portions of the MVSP area are reserved for Business Park, Commercial Business Park, and Commercial areas that welcome economic activity and job growth and provide ample space for companies to start, grow, and expand their business in the City of Menifee. Employment, commerce activity, and point-of-sale uses can locate here to support the supply chain and bring economic prosperity to local residents and the surrounding area. Complementing these uses is a Public Facility - Civic Node positioned in the southwest

portion of the site where the City can locate a fire station, transit stop for the rail line, and/or any other needed public or quasi-public use.

2.2 Land Use Designations and Planning Areas

The Specific Plan Land Use Plan described in this section will govern the location and intensity of development in Menifee Valley. The Land Use Plan divides the 590.3-acre Specific Plan area into 13 “planning areas.” A “planning area” is a defined geographic area in which this Plan establishes development standards, allowable uses, and design guidelines are uniformly applied. Refer to Section 4, *Development Standards*, for the standards that are applicable to the land uses and planning areas described on the following pages.

Residential (R) and School (S): Planning Areas 1, 2, 3, 4, 5, and 6

The Residential (R) land use encompasses 202.3 acres in Planning Areas 1, 2, 3, 4, 5, and 6. These six planning areas combined provide for up to 1,718 single-family and multi-family residential homes, as well as residential neighborhood amenities that include but are not limited to: a private recreation center, greenbelts, a dog park, and an agriculture-themed business area that could include farm/produce stands, a garden/growing area, and small commercial and non-commercial animal-keeping. Additionally, an elementary school site is a permissible use and is allowed to be developed in Planning Area 6.

The Residential (R) land use designation allows for flexibility in residential product type by allowing both single-family and multi-family homes. The Plan promotes a range of product types and lot sizes to accommodate evolving trends in buyer demand and household market preferences as the neighborhoods are designed and built out over time. A variety of single-family detached and attached residential product types and lot sizes are encouraged, as well as a variety of attached products with higher densities, such as motor court townhomes, row towns, stacked flats and other attached for-sale and for-rent units. The Residential (R) land use designation also accommodates neighborhood parks, recreation centers, greenbelts, paseos, water quality features, and other outdoor amenity areas as allowable uses so that the development of these companion uses that support and enhance an indoor/outdoor lifestyle can be integrated into neighborhood designs in Planning Areas 1 through 6.

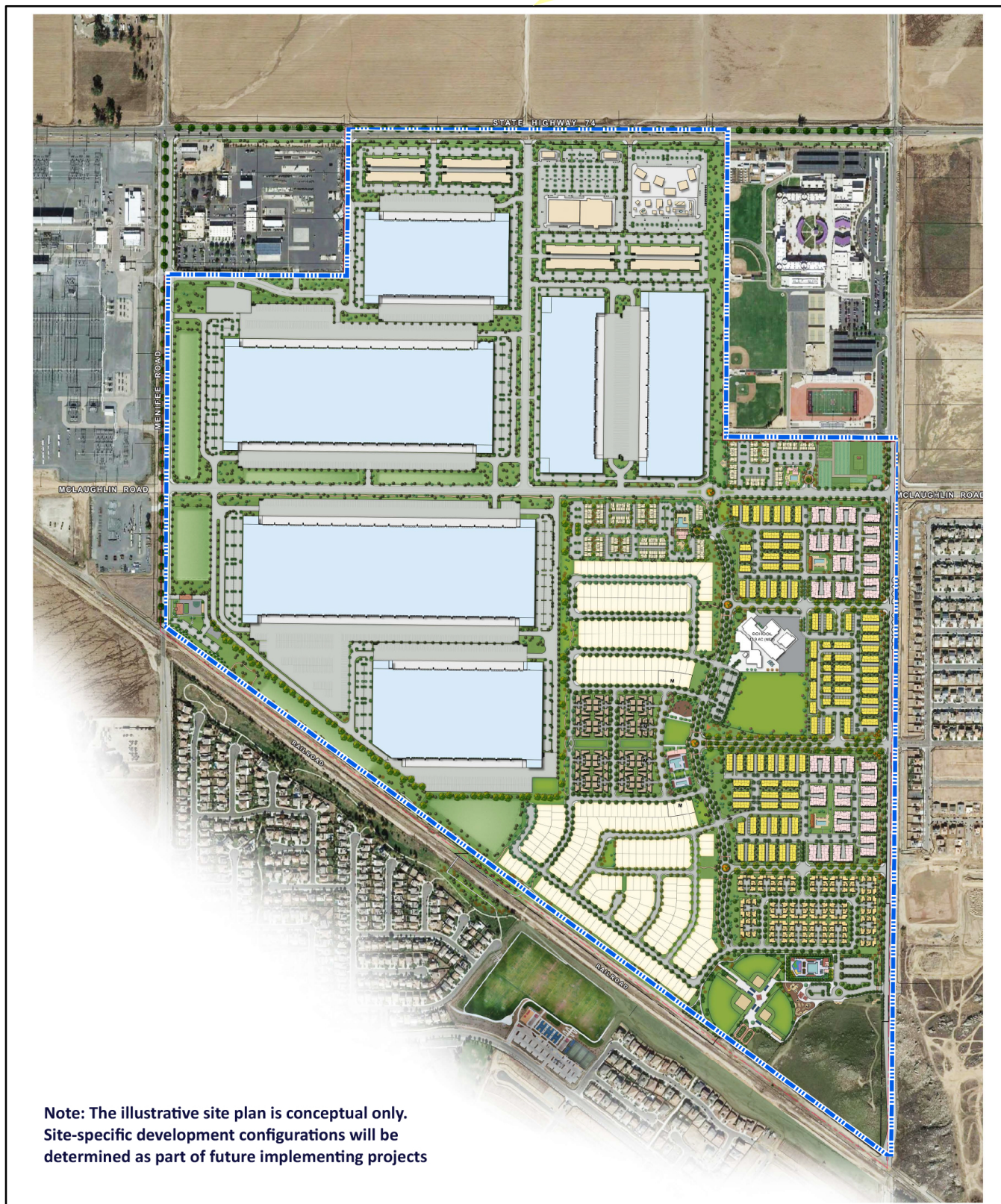
Planning Areas 1 and 3 consist of approximately 76.7 acres and can accommodate single-family detached and attached homes of varying sizes, as well as multi-family homes. Density within these planning areas is expected to vary and reach up to 20 dwelling units per acre.

Planning Areas 2, 4, and 6 comprise approximately 115.4 acres, intended for single-family detached and attached homes of varying sizes, with a density of up to 14 dwelling units per acre. A K-5 school site is allowed in Planning Area 6 on approximately 15.5 gross acres (approximately 12.0 net acres).

Table 2A - Detailed Land Use Summary

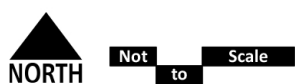
PA	LAND USE	ACRES ²	EXPECTED DENSITY RANGE (DU/AC)	TARGET DUs	TARGET DENSITY (DU/AC) ¹
1	Residential (R)	42.2	5.0-20.0	448	10.6
2	Residential (R)	61.6	4.5-14.0	294	4.8
3	Residential (R)	34.5	5.0-20.0	493	14.3
4	Residential (R)	38.3	4.5-14.0	176	4.6
5	Residential (R)	10.2	8.0-24.0	140	13.7
6	School (S) ¹	15.5	4.5-14.0	167	10.8
Residential Subtotal		202.3	--	1,718	--
7A	Recreation - Active (OS-R)	16.7	--		
7B	Conservation (OS-C)	14.7	--		
8A	Greenbelt (OS-R)	5.9	--		
8B	Greenbelt (OS-R)	7.2	--		
Open Space Subtotal		44.5	--		
			Target Building SF		Target Floor Area Ratio (FAR)
9	Public Facility - Civic Node (PF)	5.3	120,000		0.52
10	Business Park (BP)	109.9	2,300,000		0.50
11	Business Park (BP)	105.6	2,060,000		
12	Commercial - Business Park (C-BP)	47.7	1,150,000		
13	Commercial (C)	42.6	560,000		0.30
Commerce and Job Creation Subtotal		311.1	6,190,000		--
--	Backbone Public Roadways	32.4	--		
Infrastructure Subtotal		32.4	--		
SPECIFIC PLAN TOTAL		590.3			
Notes:					
1. Planning Area 6 is expected to be developed as a school site. If not desired for a school site by the Romoland School District, Planning Area 6 will be developed with residential uses as described herein.					
2. All Planning Area acreages are approximate and will be refined with implementing Tract Maps, Site Plans, and Final Maps.					





Source(s): The Galloway Group (03-20-2023)

Figure 2-2



Conceptual Illustrative Site Plan

Planning Area 5 consists of approximately 10.2 acres and is expected to accommodate attached multi-family homes with a density of up to 24 dwelling units per acre. This Planning Area abuts Heritage High School to the south and also could accommodate agri-commercial activity, as well as a potential dog park.

The residential planning areas are envisioned to include multi-purpose trails and pedestrian and bike linkages providing ease of access and connectivity throughout the area, as well as with the open space greenbelts, and public park space to the south in Planning Areas 7A, 8A, and 8B.

Open Space Recreation (OS-R) and Open Space Conservation (OS-C): Planning Areas 7A, 7B, 8A, and 8B

Planning Areas 7A, 7B, 8A, and 8B provide 29.8 acres of Open Space-Recreation (OS-R) and 14.7 acres of Open Space-Conservation (OS-C) land uses in the southern portion of the MVSP area. In total, OS-R and OS-C land uses represent 44.5 acres of the Specific Plan area.

Planning Area 7A is designated for a large public sports park designed to include ballfields, an aquatics center, playground equipment, parking, and other associated amenities. Development of the park and recreational amenities in Planning Areas 7A is to be coordinated with the City of Menifee and serve as a public park.

Planning Area 7B is designated for a passive open space area that includes the preservation of Granite Hill. The passive open space park area on and around Granite Hill is planned for limited trail development supporting passive recreation use and retention of the existing aesthetic character of the landform. No development or fuel modification is permitted within Planning Area 7B, except as necessary for the construction of Briggs Road.

Planning Areas 8A and 8B, which run along the southern boundary of the Specific Plan area, serve as a greenbelt for passive recreation, including a Class I bicycle trail with contiguous 4-foot-wide decomposed granite pedestrian trail and landscaped green space. It is from this area that a pedestrian bridge is envisioned over the railroad tracks to the south to connect the greenbelt with an existing greenbelt system in the community of Heritage Lake and provide for non-vehicular connectivity between the two developments. There are also a number of existing and proposed facilities and easements including an SCE easement for transmission lines and existing poles, easements for Riverside County Flood Control storm drain facilities, EMWD easements for water and sewer facilities, and other existing utility provider facilities in and along Planning Areas 8A and 8B.

Public Facility - Civic Node (PF): Planning Area 9

Planning Area 9 represents 5.3 acres in the southwestern corner of the Specific Plan area and is designated as a Public Facility - Civic Node (PF) site for the location of a fire station or similar

public service use. A passenger stop for trains using the rail line also could occur in this location. Up to 120,000 square feet of building space is targeted to occur in Planning Area 9. It is anticipated that this land will be dedicated to, and future development facilitated by, the City of Menifee.

Business Park (BP): Planning Areas 10 and 11

The Business Park (BP) land use is represented in Planning Areas 10 and 11 consisting of 215.5 acres. Up to 4,360,000 square feet of building space is expected to occur within these two planning areas, at a development intensity of approximately 0.50 Floor Area Ratio (FAR). Buildings are anticipated to accommodate a mixture of general light industrial, manufacturing, warehouse/storage, fulfillment center, e-commerce, and associated operations. Building sizes will vary and have the potential to be large, reaching well over 1,000,000 square feet in size.

Commercial Business Park (C-BP): Planning Area 12

The Commercial Business Park (C-BP) area consists of 47.7 acres within Planning Area 12. Up to 1,150,000 square feet of building space is expected in Planning Area 12, at a target intensity of approximately 0.50 FAR. Buildings are anticipated to accommodate business park, commercial, retail, incubator, and small-scale light industrial uses. General light industrial, manufacturing, warehouse/storage, fulfillment center, and e-commerce operations are permitted except within distances specified in this Specific Plan Section 4 to residential uses and Heritage High School.

Commercial (C): Planning Area 13

The Commercial (C) area consists of 42.6 acres within Planning Area 13. Up to 560,000 square feet of building space is anticipated in Planning Area 13, at a target intensity of 0.30 FAR. Buildings are anticipated to accommodate a mixture of commercial, retail, and incubator uses. Agri-commercial uses are also permitted in Planning Area 13.

Backbone Public Roadways

32.4 acres of the Specific Plan area will consist of right-of-way for backbone public roads, including Highway 74, Menifee Road, Briggs Road, Malaga Road, McLaughlin Road, Biscayne, and McKinley. Future right-of-way for interior public roadways would be identified as part of future implementing development permits.

Section 3

Mobility and Infrastructure



Brookfield
Properties

3 - Mobility and Infrastructure

3.1 Vehicular Circulation Plan

The Menifee Valley Specific Plan (MVSP) area is located approximately 2.5 miles east of Interstate 215 (I-215), which is the nearest freeway to Menifee Valley and part of the state highway system. I-215 links the City of Menifee to northern Riverside County to the north and San Diego County to the south. State Route 74 (SR-74, and known as "Highway 74") forms the northern border of the MVSP and serves as the main roadway connection to I-215. Highway 74 is a truck route. The segment of Menifee Road that forms the western boundary of the MVSP also is a truck route. Briggs Road forms the eastern boundary of the MVSP.

The Vehicular Circulation Plan for Menifee Valley involves frontage improvements to the perimeter roads, including Highway 74, Menifee Road, and Briggs Road. The internal street system reinforces the concept of traditional residential neighborhood design in the eastern portion of the MVSP and ease of access for business and commerce in the northern and western portions of the MVSP. The residential and non-residential components of the community only share internal vehicular access along McLaughlin Road to maximize the separation of vehicles accessing residential neighborhoods from trucks, service vehicles, and employees accessing the business park, commercial-business park, and commercial areas in the western and northern portions of the MVSP area as much as feasible. Vehicular access into and through residential neighborhoods will occur by Residential Collector Streets and the eastern segment of McLaughlin Road connecting with Briggs Road. An internal Central Spine Street is envisioned as a main internal north/south street connecting residential neighborhoods, parks, and the K-5 school site in Planning Area 6. Primary vehicular access to the Business Park, Commercial-Business Park, and Commercial areas will occur by Malaga Road and the western segment of McLaughlin Road, which connect with Highway 74 and Menifee Road.

Figure 3-1, *Vehicular Circulation Plan*, depicts public street locations and access point locations including Biscayne and McKinley. Interior roadway alignments and locations of expected traffic control devices (signals and stop signs) described in this Specific Plan are approximate until approved by the City Engineer as part of subdivision map approvals. Intersection spacing is required to confirm with the City's standard intersection spacing requirements depending upon the street's classification. Also, if any roadway rights-of-way traverse or cross McLaughlin Road, approval from, and compliance with all public utility agencies that have existing easements and facilities within the right-of-way are required. Refer to Section 5, *Landscape Guidelines*, for information about landscape in and adjacent to roadway rights-of-way.

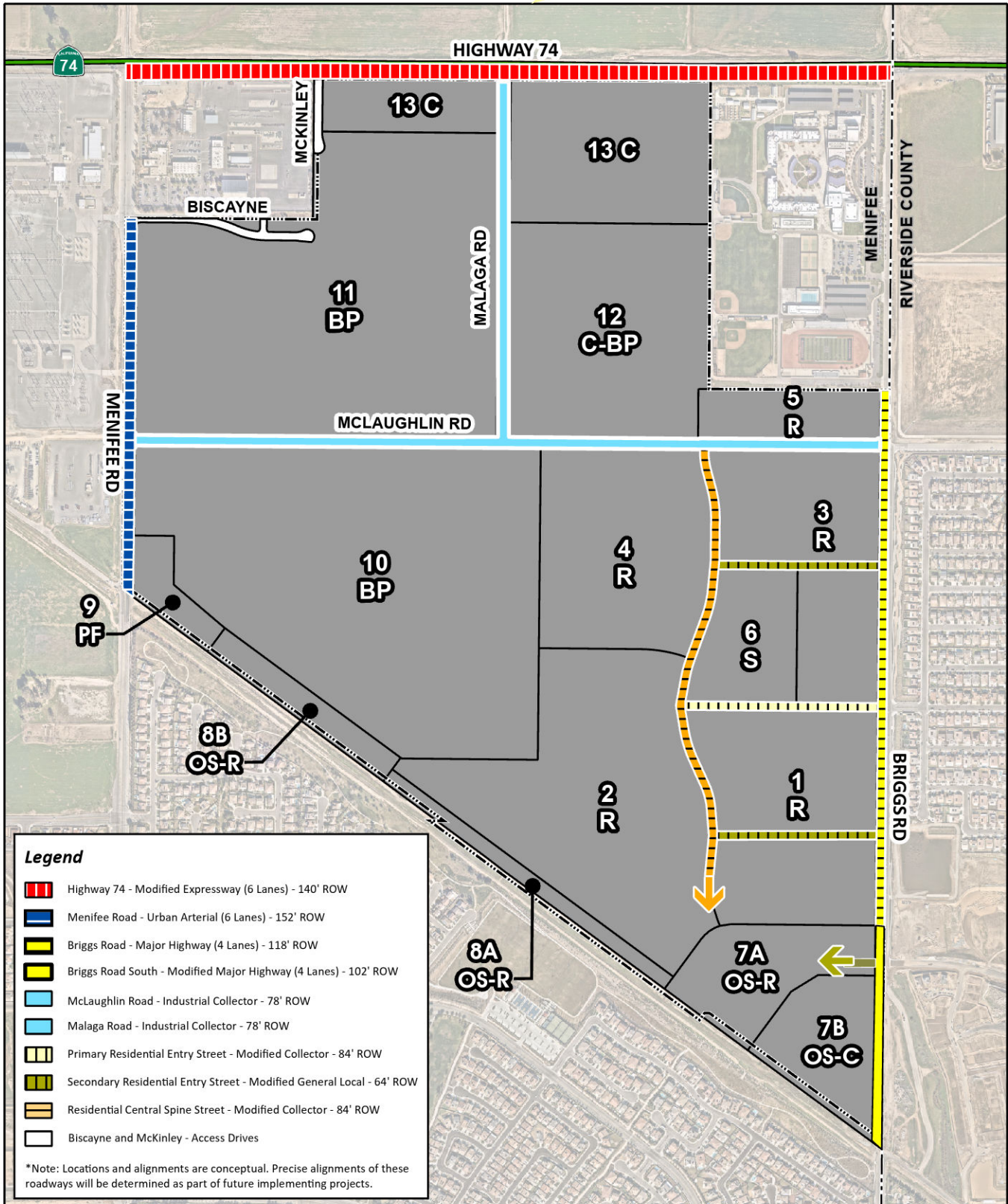
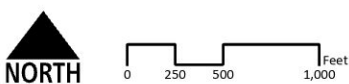


Figure 3-1

Source(s): ESRI, RCTLMA (2023), Nearmap Imagery (2023), The Galloway Group (12-10-2021), Hunsaker & Associates (05-03-2022)



3.1.1 Roadway Design Standards

The design specifications for perimeter roadways are described below and depicted on Figure 3-2, *Street Sections - SR-74, Menifee Road, Briggs Road*. Roadway alignments, designations, widths, and the cross-section details described below and depicted on Figure 3-2 are subject to change based on engineering review and approval by the City Engineer at the time subdivision maps are brought forward for approval. Refer to Section 3.2.1, *Pedestrian Mobility*, for detail on non-vehicular trails and sidewalks.

State Route 74

State Route 74 (SR-74) is classified as a 6-lane Modified Expressway, an Eligible State Scenic Highway, and Enhanced Landscape Corridor, and serves as a truck route. This classification calls for a 200-foot-wide right-of-way in the City of Menifee General Plan. As part of the MVSP, the developer of Planning Area 13 is responsible for contributing to the widening and improvement of SR-74 along Planning Area 13's frontage (110-foot-wide half width). Southerly half-width improvements are designed to comply with Caltrans requirements and include a 47-foot wide parkway inclusive of a curb-adjacent landscaped parkway of varied width, and a 6-foot-wide meandering sidewalk. Southerly half-width improvements within the roadway include the installation of new curb and gutter, curb-adjacent shoulder, and three eastbound vehicular travel lanes having a combined total width of not less than 46 feet. The center median existed as a raised center median ranging from 3-feet to 13-feet in width at the time this Specific Plan was prepared. A median break and traffic signal are expected where Malaga Road intersects with SR-74.

Menifee Road

Menifee Road is classified as a 6-lane Urban Arterial, an Eligible County Scenic Highway, and Enhanced Landscape Corridor. The segment of Menifee Road that fronts the MVSP and continues north to SR-74 and south to McCall Boulevard is also a truck route. The road classification provides for a 152-foot right-of-way in the City of Menifee General Plan. As part of the MVSP, the developers of Planning Areas 9, 10, and 11 are responsible for contributing to the widening and improvement of Menifee Road along the frontage of these planning areas. Easterly half-width improvements are designed to include a 21-foot-wide landscaped parkway inclusive of a curb-adjacent landscape area of varied width, and a 6-foot-wide meandering sidewalk. Easterly half-width improvements within the roadway include the installation of new curb and gutter, curb-adjacent shoulder, Class II bike lane, and three northbound vehicular travel lanes having a combined total width of 48 feet. Installation of a 14-foot-wide raised center median and one lane of southbound travel with shoulder on the opposite (west) side of the center median would also be required. A median break and traffic signal are expected where McLaughlin Road intersects with Menifee Road.

Briggs Road

Briggs Road is classified as a 4-lane Major Highway and an Enhanced Landscape Corridor with a 118-foot-wide right-of-way in the City of Menifee General Plan. As part of the MVSP, the developers of Planning Areas 1, 3, 5, and 7A are responsible for contributing to the widening and improvement of Briggs Road along the frontage of these planning areas. Westerly half-width improvements are designed to include a 21-foot-wide landscaped parkway inclusive of a 6-foot-wide meandering sidewalk and a landscape area of varied width. Westerly half-width improvements within the roadway include the installation of new curb and gutter, a curb-adjacent shoulder, Class II bike lane, two southbound travel lanes, and a median. The combined total half width, excluding the median, is 32 feet. A 12-foot-wide raised center median will also occur in Briggs Road.

Modifications to the standard street section including but not limited to a narrower parkway, easterly shift of the centerline, and steeper grades with retaining wall are permitted on the segment of Briggs Road that curves around Granite Hill in Planning Area 7B to preserve this topographic landform with as little disruption to the natural hill as possible. Even with these modifications, it is anticipated that a retaining wall with a height of up to 50 feet will be required to construct this section of Briggs Road.

3.1.1 Interior Residential Street Design Standards

The design standards for interior residential streets are described below and depicted on Figure 3-3, *Street Sections - Interior Residential Streets*. Roadway alignments, designations, widths, and the cross-section details described below and depicted on Figure 3-3 are subject to change based on engineering review and approval by the City Engineer at the time subdivision maps are brought forward for approval.

Primary Residential Entry Street

A Primary Residential Entry Street serves as the main access into the MVSP residential community from Briggs Road, and is expected to form a 4-way intersection with Meadow Oaks Street that services the residential community in unincorporated Riverside County located on the east side of Briggs Road. This public road is designed to have an 86-foot-wide right-of-way, with one 12-foot-wide vehicular travel lane in each direction, shoulders on each side and a raised 14-foot-wide center median. Southerly right-of-way improvements also include a 15-foot parkway area comprising a 7-foot-wide curb-adjacent landscaped parkway, a 5-foot-wide sidewalk and 3-feet of landscaped parkway beyond the sidewalk. Northerly right-of-way improvements also include a 17-foot-wide parkway with a 7-foot-wide curb-adjacent landscaped parkway and a 10-foot-wide multi-purpose trail. No on street parking will be permitted on the Primary Residential Entry Street.

Secondary Residential Entry Streets

Secondary Residential Entry Streets also provide access into Residential Planning Areas from Briggs Road. These public roads are designed to have a 69-foot-wide right-of-way consisting of a 40-foot-wide paved roadway providing a single 12-foot-wide travel lane in each direction with shoulders on each side and a Class II bike lane within the right-of-way. No center median is required. The southerly side of the right-of-way also includes a 17-foot-wide parkway with 7-foot-wide curb-adjacent landscaped parkway and a 10-foot-wide multi-purpose trail, accommodating pedestrians and bicycles. The northerly side of the right-of-way includes a 12-foot-wide parkway with 7-feet of curb-adjacent landscape containing canopy shade trees and a 5-foot-wide sidewalk. No on-street parking will be permitted on Secondary Residential Entry Streets.

Residential Central Spine Street

A north/south oriented Residential Central Spine Street is planned to connect to McLaughlin Road and provide an interior connection to the residential planning areas. This street classification provides an 84-foot-wide public right-of-way allowing for one travel lane in each direction, shoulders on both sides of the street, and a 14-foot-wide raised center median. Parkway at 15-feet in width occur on both sides of the street, with both sides containing a 5-foot-wide meandering sidewalk and a varying width curb-adjacent landscaped parkway with a minimum 6-foot-wide landscape area. Beyond the westerly public right-of-way within private property, a 10-foot-wide meandering multi-purpose trail, accommodating pedestrians and bicycles is provided within a minimum 18-foot-wide landscaped setback.

To promote traffic calming and reduce vehicle speeds on the Residential Central Spine Street, roundabouts (traffic circles) are encouraged where this street meets Primary and Secondary Residential Entry Streets, subject to design approval by the City Engineer. Figure 3-4, *Roundabout Design*, provides a conceptual illustration.

Local Residential Streets

Local Residential Streets provide access within residential neighborhoods and allow for driveway aprons to serve residential homes. This classification provides a 56-foot-wide right-of-way that allows for a 10-foot-wide travel lane and an 8-foot-wide shoulder in each direction, and a 10-foot-wide landscape parkway on each side of the street inclusive of a 5-foot-wide sidewalk.

Residential Private Drives and Alleys

Residential Private Drives and Alleys connect to internal roadways and provide access to individual garages and driveways. The Residential Private Drives classification provides a street section that allows for two 10-foot-wide travel lanes.

3.1.1 Business Park, Commercial, and Commercial-Business Park Roadway Standards

The design standards for streets serving the Business Park, Commercial-Business Park, and Commercial areas are described below and depicted on Figure 3-5, *Street Sections – Business Park and Commercial Streets*. Roadway alignments, designations, widths, and the cross-section details identified described below and shown on Figure 3-5 are subject to engineering review and approval by the City Engineer at the time subdivision maps and building plans are brought forward for approval.

Malaga Road and McLaughlin Road

Malaga Road and McLaughlin Road, designated as Industrial Collector Streets, serve as the main access into Planning Areas 10, 11, 12, and 13 from SR-74 and Menifee Road. The eastern portion of McLaughlin Road also provides access into Planning Areas 3, 4, and 5. These streets are designed to provide a 78-foot-wide right-of-way with a painted center median, one 12-foot-wide travel lane in each direction, a 10-foot-wide shoulder on each side of the street, and 11-foot-wide parkways on each side of the street inclusive of a 6-foot-wide curb-adjacent sidewalk and 5-foot-wide landscaped area. McLaughlin Road includes a Southern California Edison (SCE) easement partially within and partially outside of the right-of-way. Within and adjacent to the easement, a 16-foot-wide multi-purpose trail with a decomposed granite (DG) surface meanders in an expansive private landscape zone located beyond the right-of-way.

Private Drive Aisles

Private Drive Aisles connect individual business park, commercial-business park, and commercial building sites to industrial collector streets and provide access for passenger vehicles and trucks to access employee and visitor parking lots, truck courts, and loading dock areas. Private drive aisles will have pavement widths that range between 24 and 50 feet, with their locations and alignments determined in conjunction with the design of implementing development projects in Planning Areas 10, 11, 12, and 13. Private Drive Aisles serving truck courts, loading docks, and other portions of building sites requiring secured access may be controlled by an automatic or manually controlled security gate.

3.1.2 Bus and Rail Transit Options

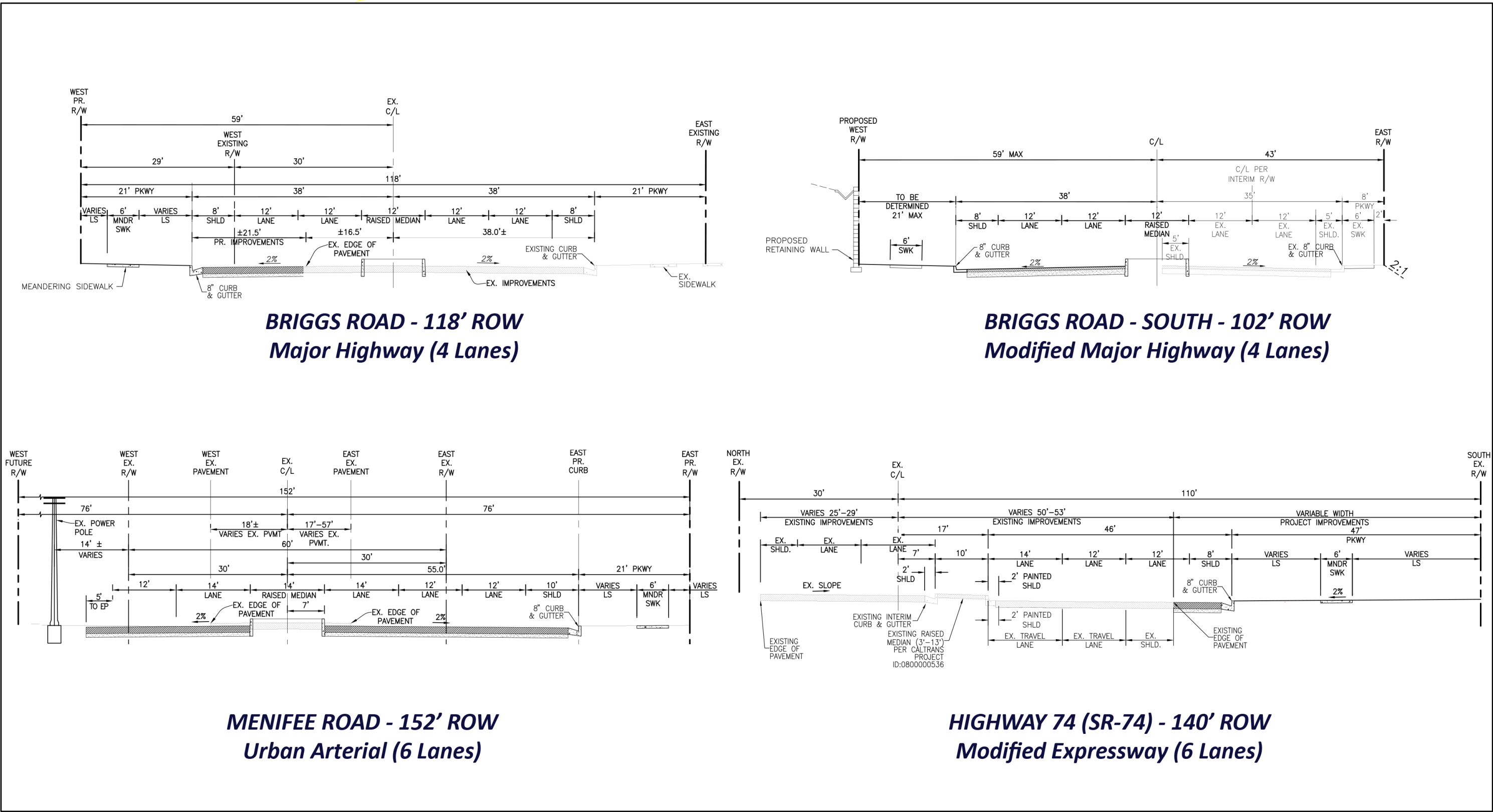
There is existing Riverside Transit Agency (RTA) service along SR-74. Also, the City of Menifee General Plan identifies potential future transit service along Menifee Road. At the time that road improvement plans or subdivision maps are filed for development adjacent to these roads, locations of bus stops or turnouts should be determined in consultation with the RTA and the City of Menifee. At a minimum, a bus turnout location, which provides a designated area on the side of the road for buses to pull out of the flow of traffic to pick up and drop off passengers, shall be provided at the intersection of Highway 74 and Malaga Road located in Planning Area 13.

Additionally, Planning Area 9 is designated as a 5.3-acre Public Facility – Civic Node (PF) site, with development in this area facilitated by the City of Menifee. Planning Area 9 is located next to a Riverside County Transportation Commission (RCTC) rail line and is the potential location of a passenger stop for trains using the rail line.

3.1.3 General Vehicular Circulation Standards

The following standards apply.

- The applicant applying for an implementing development project shall install frontage roadway improvements and traffic control measures per the recommendations of the project's Traffic Study.
- Intersection locations shall conform to the City's standard intersection and access spacing requirements for the street's classification.
- Sight distances at intersections, roundabouts, and driveway aprons shall be reviewed by the City Engineer in conjunction with the preparation of grading, landscape, and street improvement plans. This review shall ensure that setbacks allow for clear unobstructed sight distances at intersections.
- The applicant applying for an implementing development project in Planning Areas 10, 11, 12, and 13 shall demonstrate that adequate turning radii is provided for large trucks on streets and drive aisles that are adjacent to and within the project site.

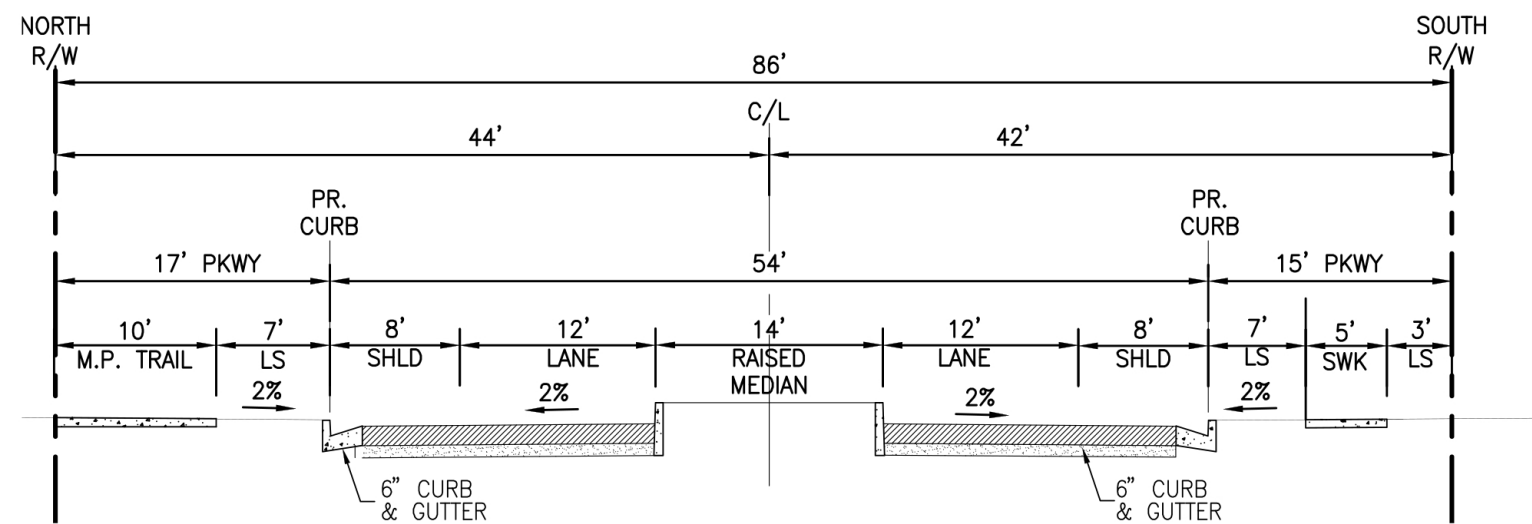


Source(s): Hunsaker & Associates (03-27-2023)

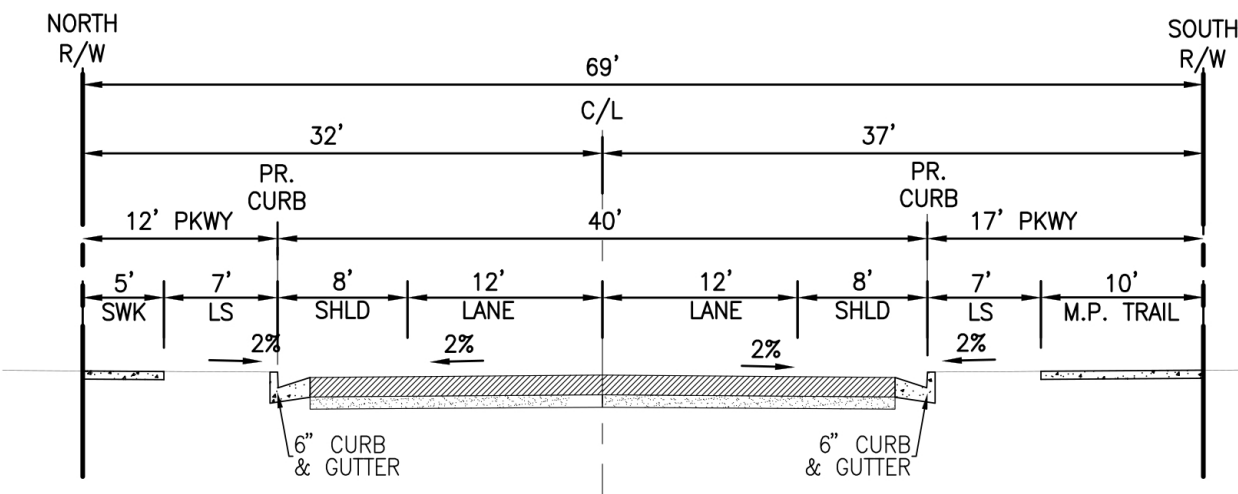
Figure 3-2

Not
to
Scale

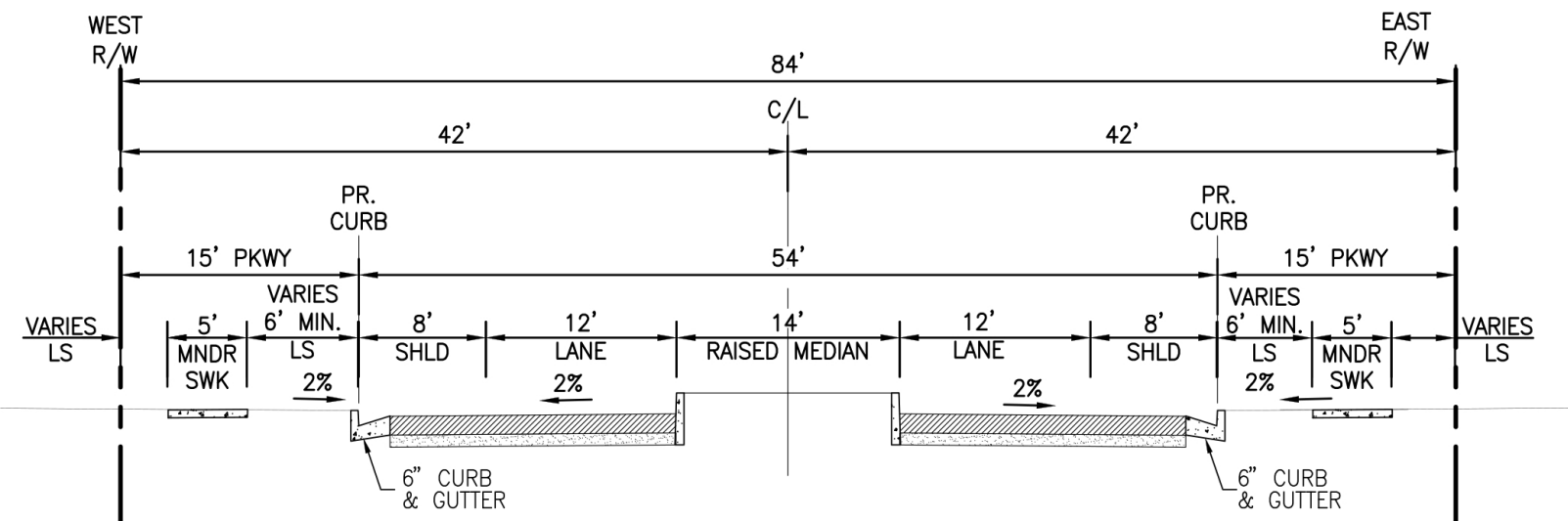
Street Sections – SR-74, Menifee Road, Briggs Road



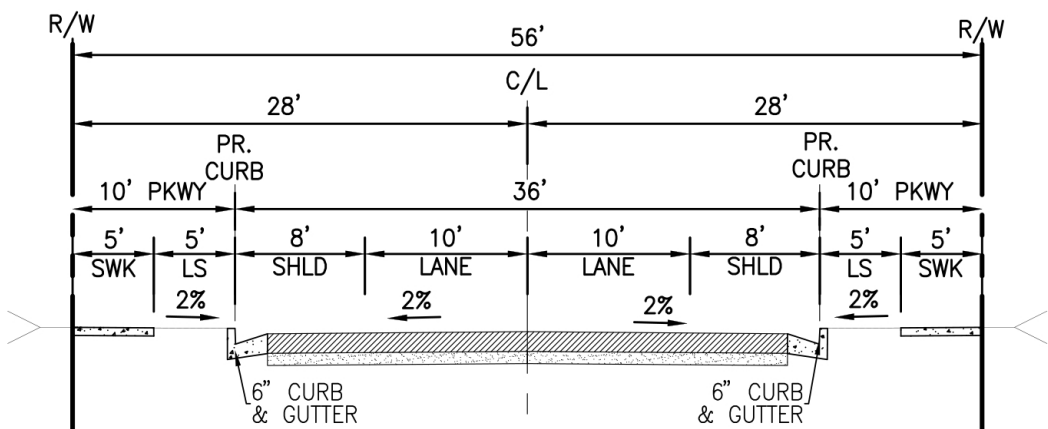
PRIMARY RESIDENTIAL ENTRY STREET - 86' ROW
Modified Collector



SECONDARY RESIDENTIAL ENTRY STREET - 69' ROW
Modified General Local



RESIDENTIAL CENTRAL SPINE STREET - 84' ROW
Modified Collector



LOCAL RESIDENTIAL STREET - 56' ROW

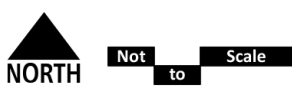
Source(s): Hunsaker & Associates (06-12-2023)

Figure 3-3



Source(s): Brookfield (September 2023)

Figure 3-4



Roundabout Design



Figure 3-5

3.2 Non-Vehicular Mobility Plan

The Menifee Valley Specific Plan is designed with a comprehensive greenbelt, sidewalk, bicycle, and multi-purpose trail system that provides ease of mobility throughout the community. Connectivity will be provided to and through the residential planning areas, as well as to the parks, amenity areas, the elementary school site, civic facilities, and to pedestrian and bicycle routes extending beyond the Specific Plan's boundary. The mobility plan also makes use of the greenbelts in Planning Areas 8A and 8B and the Granite Hill conservation area in Planning Area 7B. Sidewalk connectivity also is assured between the Business Park, Commercial Business Park, and Commercial planning areas. The non-vehicular network ensures that residents, employees, and visitors have opportunities to travel by foot and bike in different settings.

3.2.1 Pedestrian Mobility

An important element of the Specific Plan is a system of interconnecting pedestrian-friendly sidewalks and walking trails that connect neighborhoods and encourage exercise and walkability within the community and beyond its borders. This system will link the residential homes to neighborhood amenities, civic uses, and the walking trail on and around Granite Hill. The sidewalks along residential streets will provide a safe on-foot route to the K-5 school site allowed in Planning Area 6. The meandering multi-purpose roadside trail along the west side of Briggs Road will make a safe connection to Heritage High School located at the intersection of Briggs Road and Highway 74.

Connections to and from the surrounding area include sidewalks along Highway 74, Menifee Road, and Briggs Road as well as a trail system in Planning Areas 8A and 8B at the MVSP's southern border where a Pedestrian and Bike-Only bridge will cross over the rail line and connect to the greenbelt trail and open space system that occurs to the south in the community of Heritage Lake. Figure 3-6, *Pedestrian Mobility Plan*, shows the pedestrian connections envisioned throughout the Specific Plan area.

The pedestrian system primarily includes greenbelt trails, multi-purpose meandering trails, meandering sidewalks, traditional linear sidewalks, and a nature trail. The positioning and placement of the pedestrian facilities in greenbelts, roadway parkways, open space, and other areas of the community shown on Figure 3-6 and elsewhere in this Specific Plan is conceptual and the final locations will be determined at the time that implementing development projects are brought forward.

Meandering Multi-Purpose Greenbelt (DG) Trails

Meandering Multi-Purpose Greenbelt (DG) Trails are designed to occur in an area paralleling the south side of McLaughlin Road, in Open Space Recreation (OS-R) Planning Areas 8A and 8B and along the southern boundary of Public Facility-Civic Node (PF) Planning Area 9. These areas contain utility lines, subsurface water, sewer, and storm drain lines, and easements that are a constraint for structural development but an opportunity for greenbelts and passive uses such as walking and biking. These trails consist of a DG surface. A non-vehicular Pedestrian and Bike-Only Bridge in Planning Area 8A will span over the RCTC rail line and connect to the

Heritage Lake community to link the communities' trails and bike systems together, offering a more expansive and unified non-vehicular circulation system.

Meandering Multi-Purpose Roadside (DG) Trails

Meandering Multi-Purpose Roadside (DG) Trails are designed to traverse in and adjacent to roadway rights-of-way to separate walkers from vehicles and to provide an enjoyable mobility path that is more inviting and welcoming to outdoor exercise than a traditional sidewalk. These trails consist of a DG surface, are 10 feet wide, and can occur inside or outside of the public roadway right-of-way or weave partially in and partially out of the right-of-way extending into privately-owned green space that parallels the road.

Meandering Multi-Purpose Roadside (DG) Trails are planned in the following locations:

- West side of Briggs Road, outside of the right-of-way.
- West side of Residential Central Spine Street, outside of the right of way.

Non-Meandering Multi-Purpose Roadside (DG) Trails

Non-Meandering Multi-Purpose Roadside (DG) Trails are designed to traverse in and adjacent to roadway rights-of-way to separate walkers from vehicles and to provide an enjoyable mobility path that is more inviting and welcoming to outdoor exercise than a traditional sidewalk. These trails consist of a DG surface, are 10 feet wide, and can occur inside or outside of the public roadway right-of-way or be located partially in and partially out of the right-of-way extending into privately-owned green space that parallels the road.

Non-Meandering Multi-Purpose Roadside (DG) Trails are planned in the following locations:

- North side of the Primary Residential Entry Street, in the right-of-way
- South side of Secondary Residential Entry Streets, in the right-of-way

Meandering Sidewalks

Meandering Sidewalks that are non-contiguous to roadway curbs are located in the public right-of-way and provide a pleasing walking environment that separates pedestrians from vehicular travel. This sidewalk type ranges from 5 feet to 6 feet in width.

Meandering Sidewalks are planned in the following locations:

- South side of Highway 74 (6 feet wide) fronting Planning Area 13
- West side of Briggs Road (6 feet wide)
- East side of Menifee Road (6 feet wide)
- Both sides of the Residential Central Spine Street (5 feet wide)

Linear Sidewalks

Traditional, linear, curb-adjacent sidewalks are provided along roads in the Business Park, Commercial-Business Park, and Commercial areas where the roadway parkways are more utilitarian than recreational. Linear sidewalks are also provided in several of the Residential areas. These sidewalks range from 5-feet to 6-feet wide.

Linear Sidewalks are planned in the following locations:

- Both sides of Malaga Road (6 feet wide).
- Both sides of McLaughlin Road (6 feet wide).
- South side of the Primary Residential Entry Street (5 feet wide)
- North side of Secondary Residential Entry Streets (5 feet wide)
- Both sides of Local Residential Streets (5 feet wide)
- North boundary of Planning Area 7A (5 feet wide)

Nature Trail

A Nature Trail is planned to be accommodated on and around Granite Hill in the public passive park area of Planning Area 7B where a trail already existed when this Specific Plan was prepared. The trail, if provided, must meet public safety standards and methods such as signage and physical deterrents should be strategically positioned to keep trail users on the trail and away from environmentally sensitive areas of Granite Hill.

3.2.2 Bicycle Mobility

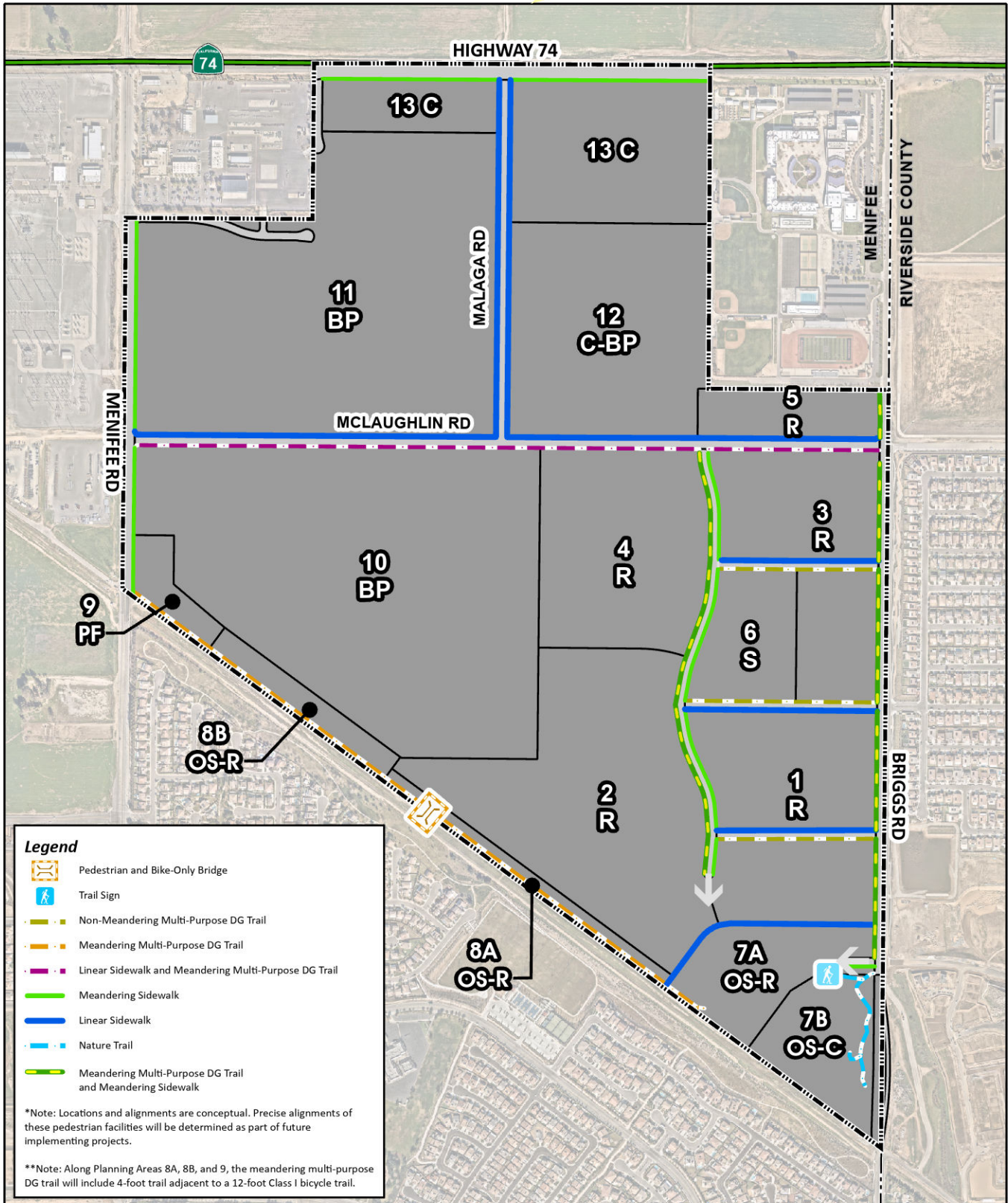
Bicycle travel routes are provided in Menifee Valley and connect to the City of Menifee's bikeway network by means of informal bicycle accommodations in Meandering Multi-Purpose Greenbelt (DG) Trails (described above), informal bicycle accommodations in Meandering Multi-Purpose Roadside Trails (described above), in a Class I bike lane along the southern boundary of Planning Areas 7A, 8A, 8B, and 9, and in Class II bike lanes on roadways. Class II bike lanes are planned within the right-of-way along the east side of Menifee Road, west side of Briggs Road, to be striped as part of the roadway improvements along the frontage of Planning Areas 1, 3, 5, 7A, 7B, 9, 10, and 11. Class II bike lanes are also planned within the right-of-way on both sides of the Primary Residential Entry Street, Secondary Residential Entry Streets, and Residential Central Spine Street. Class III bike routes share travel lanes with vehicles and are planned within the right-of-way along Malaga and McLaughlin Roads and in Local Residential Streets. Bicycle travel along interior residential streets, Malaga Road, and McLaughlin Road that do not offer an off-road option will share the road with vehicles and follow all requirements for safe bicycle circulation. Also, Business Park, Commercial-Business Park, Commercial, School, and Park uses will provide bicycle racks and/or storage facilities as required by the California Green Building Code (CALGreen). Figure 3-7, *Bicycle Mobility Plan*, reflects these connections throughout the Specific Plan area.

3.2.3 General Non-Vehicular Circulation Standards

The following standards apply.

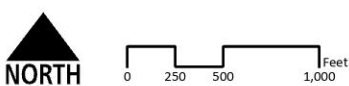
- Pedestrian and bicycle facilities within the Specific Plan boundary and constructed consistent with the specifications described in this Specific Plan are considered approved by the City Engineer, and consistent with City standards.

Pedestrian crosswalks indicated by pavement markings shall be provided where required by the City Engineer.



Source(s): ESRI, RCTLMA (2023), Nearmap Imagery (2023), Hunsaker & Associates (05-03-2022)

Figure 3-6



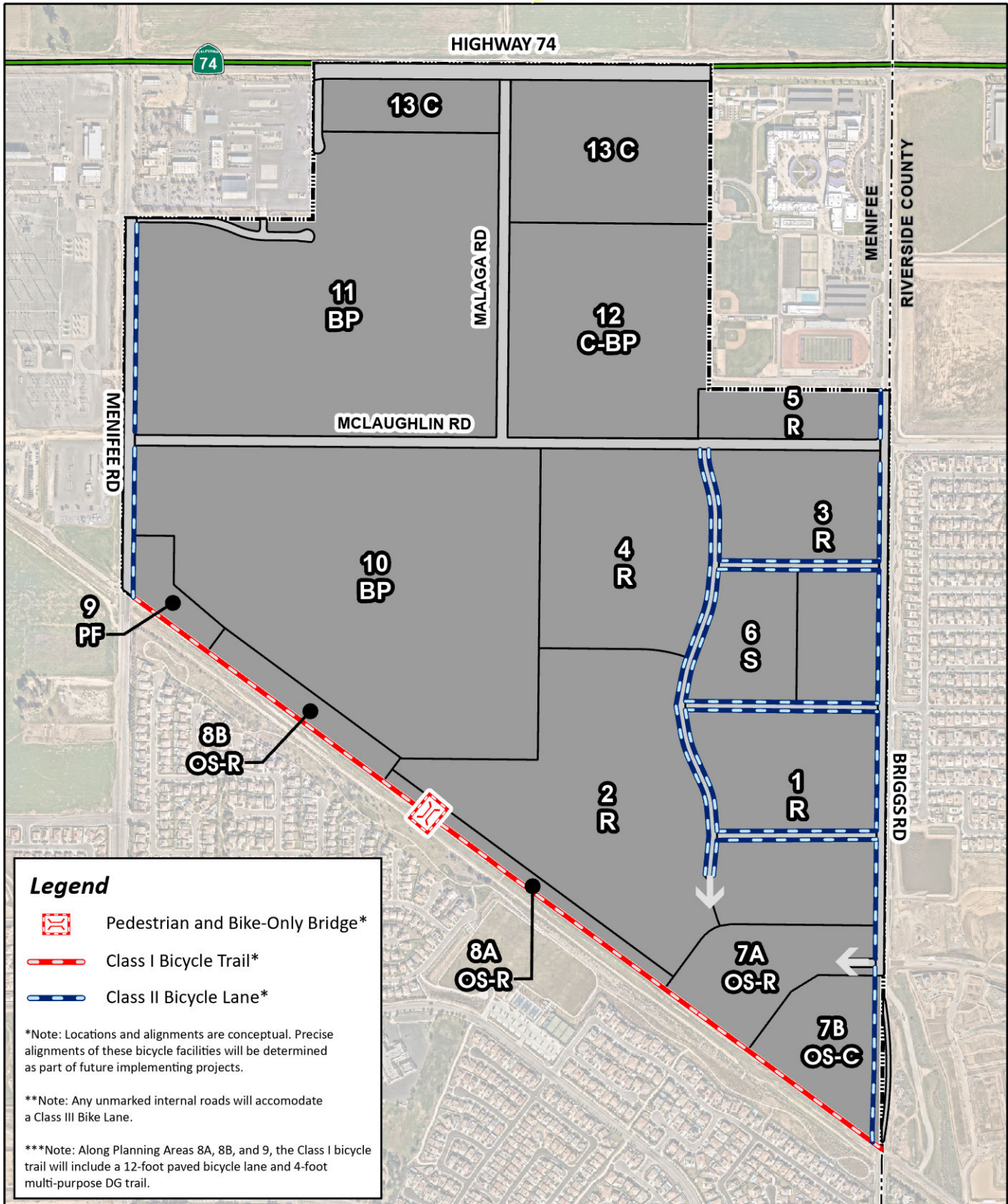
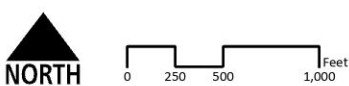


Figure 3-7

Source(s): ESRI, RCTLMA (2022), Nearmap (2022), The Galloway Group (12-10-2021), Hunsaker & Associates (05-03-2022)



3.3 Water Plan

3.3.1 Domestic Water Plan

Eastern Municipal Water District (EMWD) provides domestic water service to the area. Domestic water provided by EMWD is served with a blend of the California State Water Project and Colorado River waters, imported, and supplied to EMWD by the Metropolitan Water District (MWD).

The Domestic Water Plan provides the backbone water infrastructure to service the Menifee Valley community, as shown on Figure 3-8, *Conceptual Domestic Water Plan*. Adequate water service can be provided for the Specific Plan area using existing and planned facilities. The Specific Plan area will be served by EMWD in the 1627 and 1720 pressure zones and the existing Longview water tank.

Existing 12-inch diameter water pipelines in the 1720 pressure zone are located within a portion of SR-74 and a portion of Briggs Road. Existing 12-inch to 30-inch diameter water pipelines in the 1627 pressure zone are located in a portion of Menifee Road, in SR-74 west of Menifee Road, and in a portion of McLaughlin Road. To provide potable water to the Specific Plan's land uses, development projects in the Specific Plan area will be required to install domestic water lines ranging from 8-inches to 18-inches in diameter to connect with the existing facilities in the 1720 pressure zone. The locations of backbone lines are shown in Figure 3-8. These include on-site lines and an off-site line that will extend in Menifee Road from McLaughlin Road, north to SR-74, and beyond SR-74 to Watson Road. At Watson Road, this off-site line will tie into a 30-inch pipe that is planned to be installed by EMWD in Watson Road included with the future construction of the Matthews Booster Station project.

To provide a reliable source of water for firefighting purposes, potable water is also delivered to all fire hydrants and fire sprinkler systems utilizing the potable water system. Thus, piping facilities for potable water are designed to accommodate both the domestic demand and the firefighting demand. Business Park buildings are expected to have Emergency Suppression Fast Response (ESFR) fire sprinkler systems.

3.3.2 Reclaimed Water Plan

Parks and common landscape areas are prime candidates for reclaimed water use. EMWD provides reclaimed water service to the Specific Plan area by an existing 48-inch reclaimed water line that parallels the southern boundary of the MVSP. The Specific Plan's Reclaimed Water Plan includes the installation of 8-inch to 18-inch diameter reclaimed water lines in Briggs Road, Menifee Road, and McLaughlin Road to form a loop with the existing line. To provide reclaimed water to irrigate parks and common landscaped areas, developments will be required to install reclaimed water lines to connect with this looped system. The reclaimed water will provide irrigation water to serve parks and common landscape areas as depicted in Figure 3-9, *Conceptual Reclaimed Water Plan*.

3.3.3 General Water Plan Standards

The following standards apply to the water system.

- All potable and reclaimed water lines shall be placed underground.
- All potable and reclaimed water lines and facilities shall be owned and maintained by EMWD, and designed in accordance with the requirements of the EMWD. Facilities dedicated to and maintained by EMWD shall be placed within public utility easements or public rights-of-way, which shall include rights for EMWD for access and maintenance.
- The potable water distribution system shall provide adequate flows for fire protection in accordance with requirements of the County of Riverside Fire Department.
- All water mains and fire hydrants providing required fire flows shall be constructed in accordance with the City of Menifee's standards, subject to approval by the Riverside County Fire Department.

3.4 Sanitary Sewer Plan

3.4.1 Description

EMWD provides sanitary sewer conveyance and treatment services to the Specific Plan area. The Sewer Plan is depicted in Figure 3-10, *Conceptual Sewer Plan*. Existing lines are located in Menifee Road and Briggs Road and in easements located along the southern boundary of the Specific Plan area, in McLaughlin Road, and in Planning Area 7A and 7B (Open Space). The existing lines range from 12-inches to 24-inches in diameter. To provide sewer service to land uses in the MVSP, pipes ranging from 8-inches to 15-inches in diameter will be installed with connection points to existing facilities.

3.4.2 General Sewer Plan Standards

The following standards apply to the sewer system.

- All sewer lines shall be placed underground.
- All sewer lines and facilities shall be owned and maintained by EMWD, and designed in accordance with the requirements of the EMWD. Facilities dedicated to and maintained by EMWD shall be placed within public utility easements or public rights-of-way, which shall include rights for EMWD for access and maintenance.

3.5 Stormwater Drainage Plan

3.5.1 Description

The Specific Plan area is located within the San Jacinto River Watershed, which is a sub-watershed of the Santa Ana River Watershed. Specifically, the MVSP area is located within the

Romoland/Homeland Master Drainage Plan (MDP). The MDP outlines a master plan for orderly development of flood control facilities for ultimate build-out of the area. Because the City of Menifee was not at build-out conditions when this Specific Plan was prepared, the stormwater drainage systems in the City were in differing stages of interim- and ultimate-conditions.

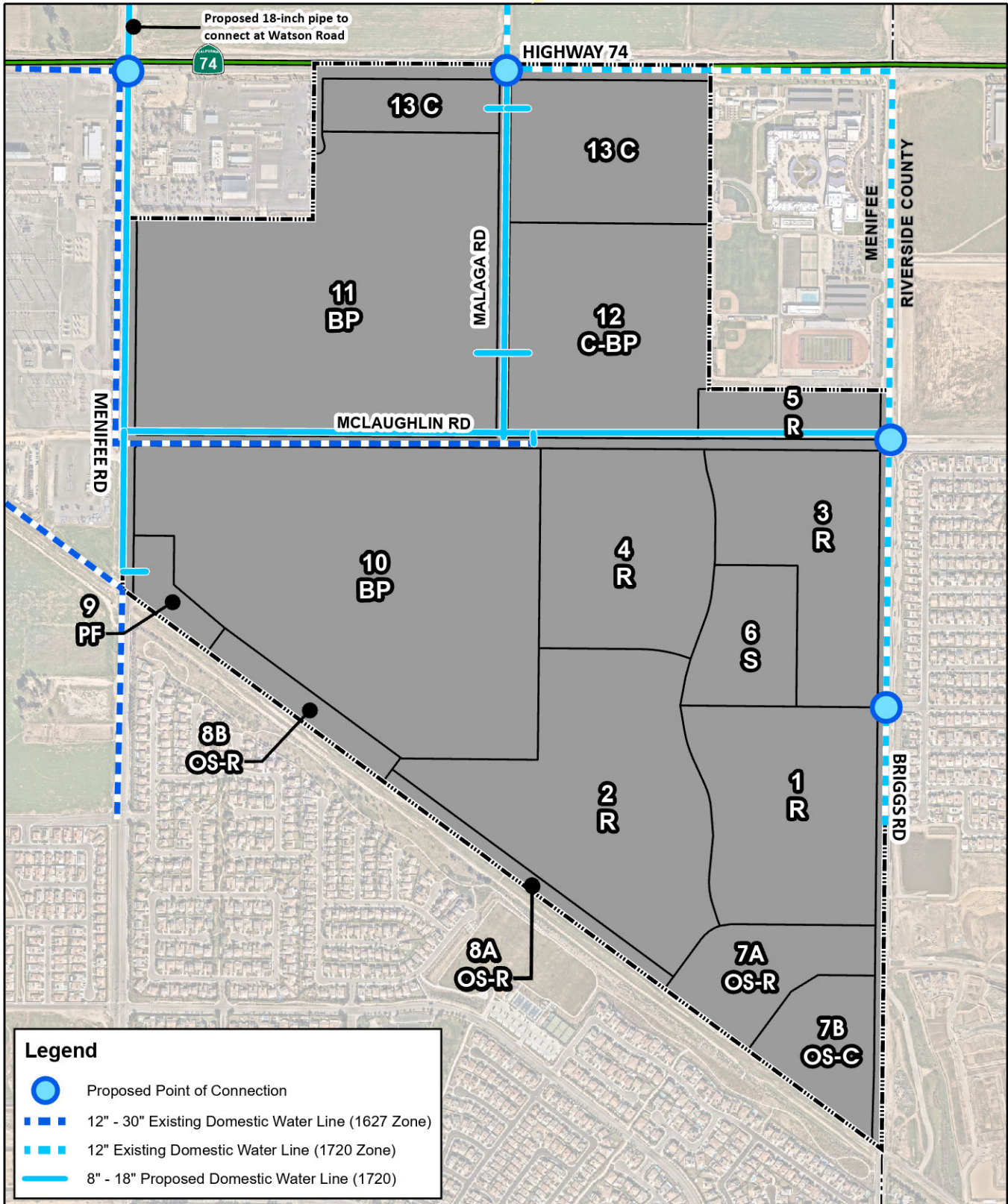
The Romoland MDP's Line A traverses McLaughlin Road and ranges in size from a 96-inch reinforced concrete pipe to an 8-foot by 10-foot reinforced concrete box. To provide service to land uses in the Specific Plan area, all proposed storm drain lines ranging in size from 18 to 102 inches in diameter will connect to the existing storm drain Line A as depicted in Figure 3-11, *Conceptual Storm Drain and Water Quality Plan*. This will include Line A-4, Line A-5, and Line A-7 as depicted on the Romoland MDP.

To capture, convey, and properly detain storm water runoff, a system of on-site surface and underground storm detention facilities will be incorporated and designed to accommodate projected storm water volumes. For large storm events, water could be permitted to be held in the Business Park truck courts as approved by the City Engineer. The storm detention facilities are to include water quality treatment measures necessary to properly address National Pollution Discharge Elimination System (NPDES) and Water Quality Management Plan (WQMP) requirements.

3.5.2 General Stormwater Drainage Plan Requirements

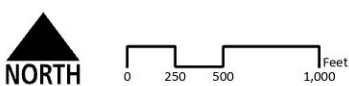
The following standards apply to the stormwater drainage system.

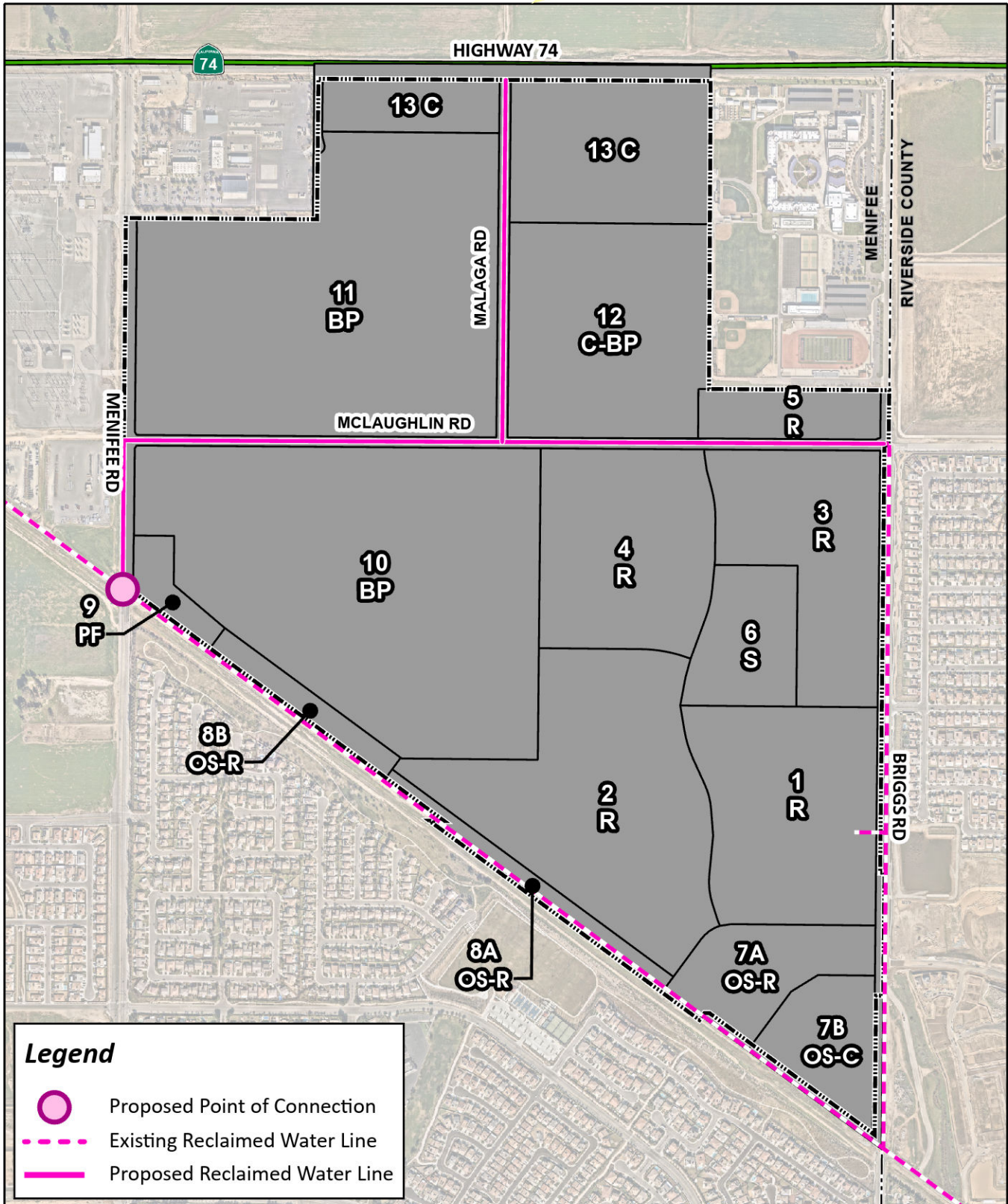
- All drainage and storm drain facilities shall be maintained by the Riverside County Flood Control and Water Conservation District, (RCFCWCD), City of Menifee, a homeowners' association, master property owners' association, and/or other maintenance entity.
- All water quality basins and water quality treatment chambers or other natural or mechanical means of water quality shall be maintained by a homeowners' association, master property owners' association, and/or other maintenance entity with the exception of facilities located on individual business park lots that only service said lot, which shall be maintained by the property owner or building tenant.
- Storm drain facilities of 36-inch diameter or less shall be maintained by the City of Menifee, a homeowners' association, master property owners' association, and/or other maintenance entity; storm drain facilities greater than 36-inch diameter shall be maintained by the RCFCWCD unless agreed upon otherwise by the City of Menifee or RCFCWCD.



Source(s): ESRI, RCTLMA (2023), Nearmap Imagery (2023), The Galloway Group (12-10-2021), Hunsaker & Associates (05-03-2022)

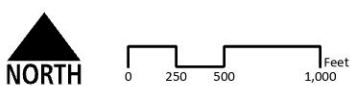
Figure 3-8



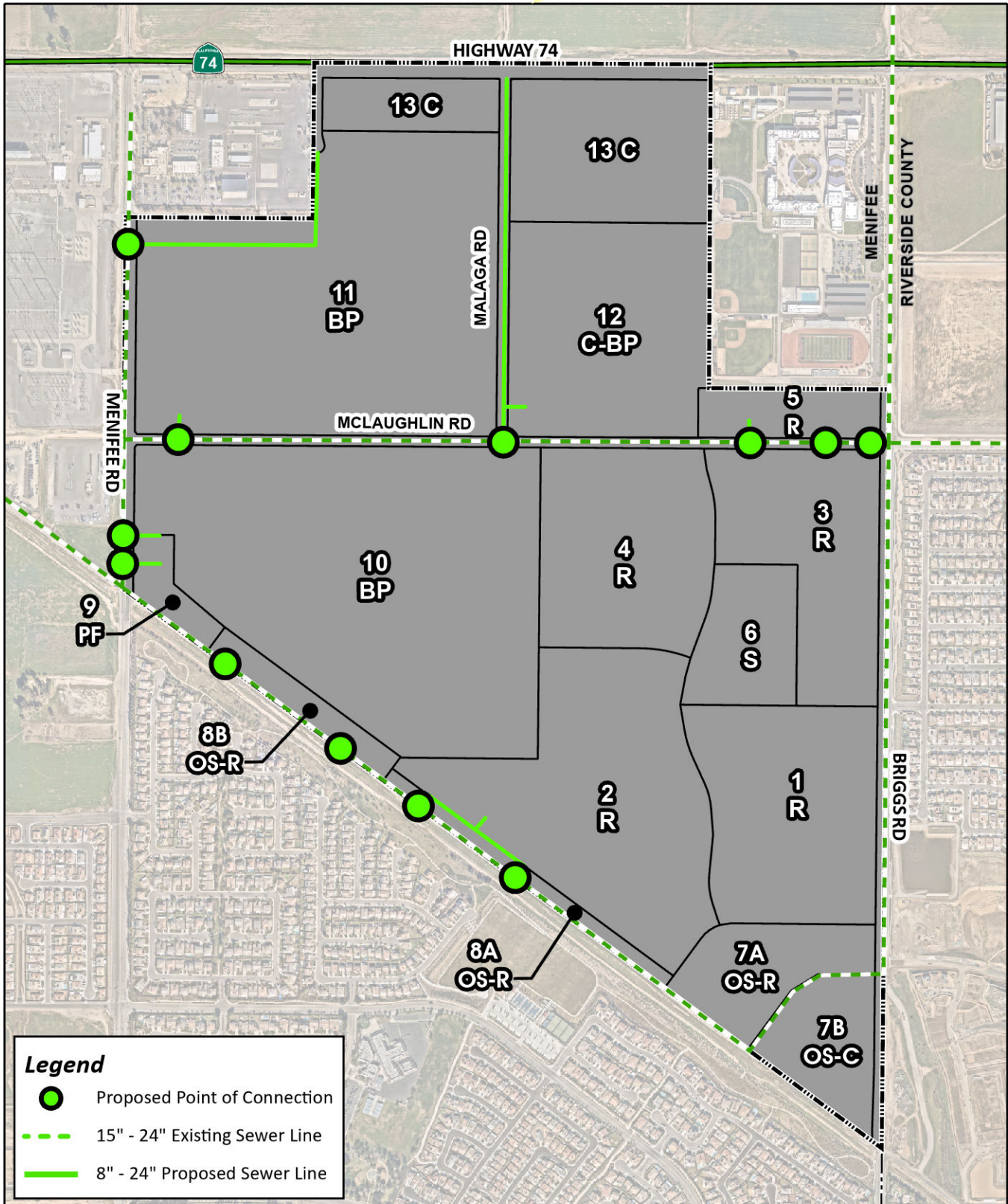


Source(s): ESRI, RCTLMA (2023), Nearmap (2023), The Galloway Group (12-10-2021), Hunsaker & Associates (05-03-2022)

Figure 3-9



Conceptual Reclaimed Water Plan



Source(s): ESRI, RCTLMA (2023), Nearmap Imagery (2023), The Galloway Group (12-10-2021), Hunsaker & Associates (05-03-2022)

Figure 3-10



Conceptual Sewer Plan



Source(s): ESRI, RCTLMA (2023), Nearmap Imagery (2023), The Galloway Group (12-10-2021), Hunsaker & Associates (05-03-2022)

Figure 3-11

**Conceptual Storm Drain
and Water Quality Plan**

3.6 Electricity, Fiber, and Natural Gas Utilities

The Specific Plan area is located within the service area of Southern California Edison (SCE) for electrical service and Verizon for telephone and internet service. Both utilities can fully serve the Specific Plan area. Additional internet providers are also available. All new on-site electrical and fiber cables will be placed underground. Redundancies in the fiber network may be necessary to service any substantial technological investments made by tenants in Business Park and Commercial Business Park buildings. Natural gas is expected to be provided to the residential, recreational, civic, and commercial land uses in the Specific Plan area. Natural gas is not typically needed in business park buildings, but can be provided on a case-by-case basis in Planning Areas 10, 11, and 12.

3.7 Public Services and Facilities

3.7.1 Schools

School-aged residents of the Menifee Valley community would be served by the Romoland School District for grades K-8 and the Perris Union High School District (PUHSD) for grades 9-12. Elementary school students (grades K-5) residing within the Specific Plan area are within the attendance boundary of Boulder Ridge Elementary School, approximately 1.7 miles southwest of the Specific Plan area, until and unless such time as an elementary school opens in the Specific Plan if needed and desired by the Romoland School District. A K-5 school is allowed to be located in Planning Area 6. Any potential school shall comply with Title 5 of the California Code of Regulations (CCR) and shall be sited in accordance with CCR Title 5, Division 1, Chapter 13, Subchapter 1, Article 2.

Middle school students (grades 6-8) are in the attendance boundary of Ethan A. Chase Middle School, approximately 2.1 miles west of the Specific Plan area. High school students (grades 9-12) will attend Heritage High School, which is located immediately east of the Specific Plan's northeast boundary at the intersection of Briggs Road and Highway 74. Additional schools to serve the Menifee Valley Specific Plan and the surrounding area may be built in the future as demand and funding allows.

Developers in the MVSP area will be required to offset effects on schools and school districts through the execution of a school facilities mitigation agreement, which may call for conveyance of land or payment of school impact fees that are set and collected by each school district in addition to ongoing property taxes.

3.7.2 Police and Fire Service

The City of Menifee is served by the Menifee Police Department, which began operations in July 2020. The closest police station to the Specific Plan is located approximately 6.3 miles southwest at 29714 Haun Road, in the City of Menifee. The Specific Plan area is within the ME100 (Beat 1) geographic patrol area.

The City of Menifee contracts all fire protection services through the Riverside County Fire Department. There are four fire stations in the City of Menifee and each station has a paramedic engine company. Fire Station #76, *Menifee Lakes*, is the closest fire station within the City of Menifee boundaries to the Specific Plan, and is located approximately 4.5 miles south of the Specific Plan at 29950 Menifee Road.

The Specific Plan includes Planning Area 9, an approximately 5.3-acre site at Menifee Road in the southwestern corner of the Specific Plan area for the establishment of civic uses, which could include a fire station and/or other emergency services facility among other civic uses.

3.7.3 Public Parks and Recreation

The MVSP area is located within proximity of several regional recreational facilities including Lake Perris State Recreational Area (approximately 9.0 miles north), Roy W. Kabian Memorial Park (approximately 6.0 miles west), and Skinner Reservoir, also known as Lake Skinner (approximately 10.0 miles south). There are also several public parks and recreational facilities located within the City of Menifee including the Marion V. Ashley Park and Community Center, Eller Park, McCall Canyon Park, and Rancho Ramona Park; all which are within an approximately 3.5-mile radius of the Specific Plan area. Additionally, the MVSP area is located immediately north of the Heritage Lake master-planned community which devotes approximately 168 acres to open space, lake, parks, paseos, and greenbelts areas, which will be connected to Menifee Valley by a planned non-vehicular bridge spanning the rail line that divides the two communities.

One public park is planned in the Specific Plan area, consisting of the public sports park in Planning Area 7A and passive open space trails in Planning Area 7B. The sports park shall develop in accordance with the City's Park standards and guidelines pursuant to Municipal Code Chapters 9.195 and 15.04.

The Planning Area 7A park is conceptually designed to include the following features:

- baseball field
- little league field
- hard surface sport courts
- sensory garden
- picnic shelters
- playground
- junior-sized Olympic pool
- splash pad
- parking area
- restroom
- nature trail
- sports field lighting
- security camera system

In addition, it is expected that passive recreational amenities will be provided in the community's greenbelts and that recreational uses also would be available at the K-5 school site that could be developed in Planning Area 6. A private recreation center could be included within the residential Planning Areas and a dog park may be included in Planning Area 5, as

well as an agri-commercial area that could be included and would have the potential to include a community garden. Refer to Section 5, *Landscape Design Guidelines*, for conceptual illustrations of the public park, private recreation center, and other recreational amenities.

3.8 Conceptual Grading Plan

The purpose of grading is to prepare lots, roads, drainage, and water quality features for development, considering the need for flat development pads and roads that meet City standards. Figure 3-12, *Conceptual Grading Plan*, represents the proposed grading concept for the Specific Plan area. Up to approximately 5.5 million cubic yards of material is expected to be moved overall (total estimated cut and fill) to achieve a balanced site including remedial grading. This quantity may vary as final grading plans are developed. Grading may occur in phases as development evolves and phase-specific grading plans will be required to undergo City review and approval.

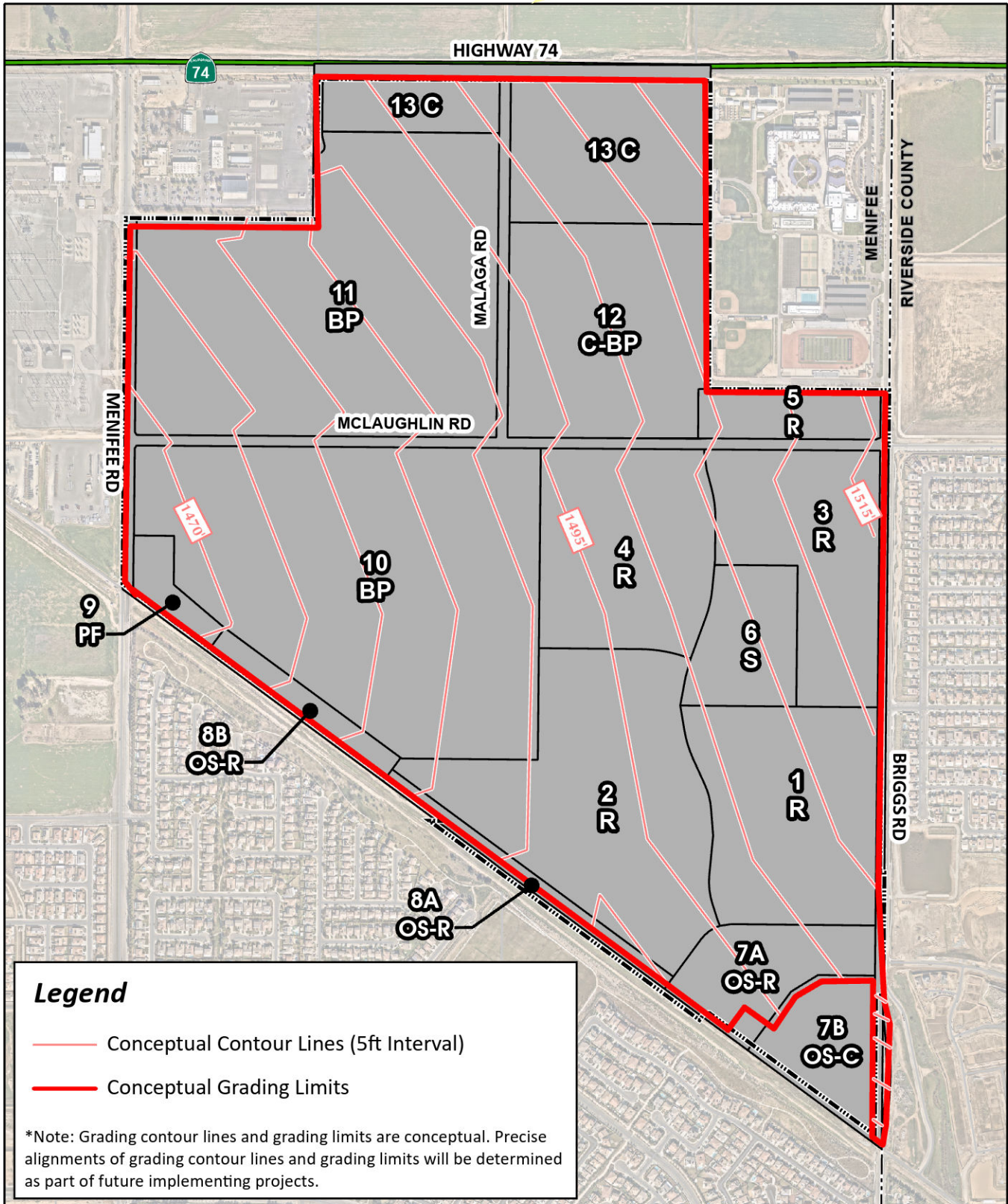


Figure 3-12



Section 4

Development

Standards



Brookfield
Properties

4 - Development Standards

4.1 Purpose and Intent

This section formally establishes the use permissions and development regulations for land uses in the MVSP. These standards serve as zoning for the MVSP in a regulatory context.

Accompanying these development standards are design guidelines for landscape and architecture, which are contained in Section 5, *Landscape Design Guidelines* and Section 6, *Architectural Design Guidelines*. This Section 4 serves as zoning and provides the framework upon which subsequent implementation and planning decisions are based. These standards also serve in part as the basis and criteria for determining consistency of site-specific design with the Specific Plan objectives.

4.2 Applicability

The regulations set forth in this Section 4 shall apply to all development plans or agreements, tract or parcel maps, plot plans, use permits, variances, or any other action requiring administrative or discretionary approval within the MVSP area. Whenever the development standards contained herein differ from those contained in the City of Menifee Development Code, the provisions of this Specific Plan shall take precedence. Any development standard, condition, or situation not specifically addressed herein shall be subject to the applicable requirements of the City of Menifee Development Code. To ensure the orderly development of the Residential, Recreational, Public Facility – Civic Node, Business Park, Commercial Business Park, and Commercial uses set forth by this Specific Plan, development standards are established herein and discussed in further detail in the sections below.

4.3 Definition of Terms

The meaning of words, phrases, titles, and terms shall be the same as provided in the City of Menifee Development Code, unless otherwise specifically defined in this Specific Plan, either below or elsewhere in this document.

Floor Area, Net. The total of all floor areas of a building, excluding stairwells and elevator shafts, equipment rooms, interior vehicular parking or loading, lofts or mezzanines of warehouse buildings used for equipment and conveyer systems, and all floors below the first or ground floor, except when used or intended to be used for human habitation.

Building Coverage: The area of a parcel covered by a structure or structures, expressed as a percentage of the total lot area.

Guest Parking: Parking spaces for multi-family developments that are openly available for visitors to the property.

Landscape Coverage. Area composed of both irrigated and non-irrigated open space areas without impervious cover that generally consist of plantings for grasses, shrubs, trees, natural unimproved ground cover, or similar living plant material. This shall include any pervious areas, such as basins designated for stormwater management or stormwater quality treatment. Non-living ornamental features such as artificial turf, boulders, gravel, dry stream beds, and similar features included as part of a xeriscaping scheme may all comprise landscape coverage.

Open Space Coverage: Area of a lot not covered by a principal or accessory structure and intended for outdoor use, living, and/or recreation.

Private Open Space: The space abutting an individual residential dwelling unit is for the private use of the inhabitant of the adjacent residential unit. For example, private open space can include but not be limited to porches, balconies, verandas, patios, and yards.

Street Trees. All trees located within a public or private street right-of-way and within an immediately adjacent landscaped setback.

4.4 Land Uses

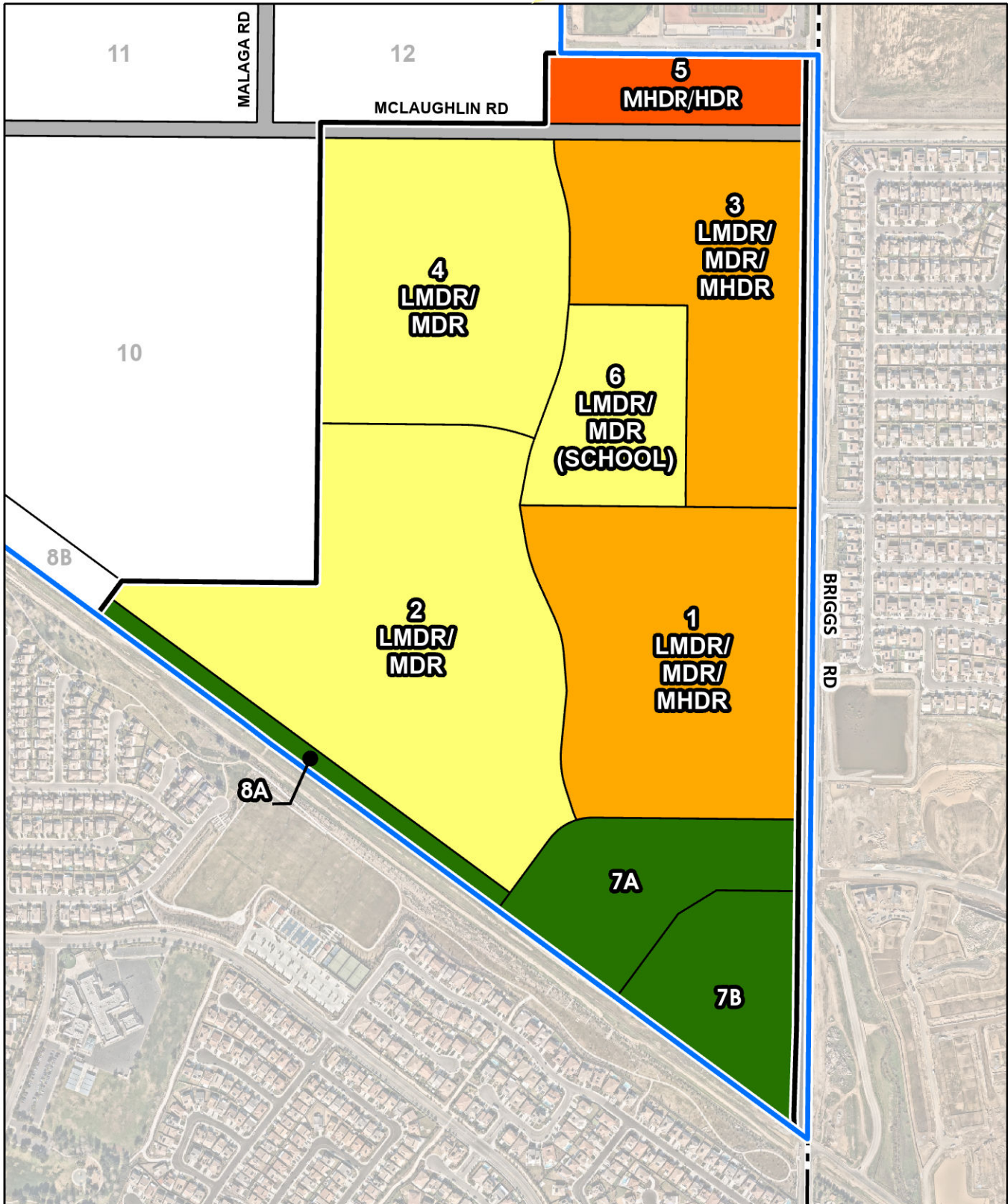
The MVSP represents a mix of Residential, Recreational, Conservation, Public Facility-Civic Node, Business Park, Commercial Business Park, and Commercial land use types. These land use types are generally described in Table 4A below. Residential land use types are illustrated in Figure 4-1, *Residential District Land Use Plan*.

TABLE 4A - Land Use Descriptions¹

PLANNING AREA	LAND USE	GENERAL DESCRIPTION
1, 2, 3, and 4	Low Medium Density Residential (LMDR)	Single Family: (SFR) Single-family attached and detached residences with a density range of 4.5 to 8 dwelling units per acre.
1, 2, 3, and 4	Medium Density Residential (MDR)	Single Family: (SFR) Single-family attached and detached residences, including townhomes, stacked flats, courtyard homes, patio homes, and zero lot line homes, with a density range of 8 to 14 dwelling units per acre.
1, 3, and 5	Medium High Density Residential (MHDR)	Single Family: (SFR) Single-family attached and detached residences with a density range of 14 to 20 dwelling units per acre.
		Small-Scale Multi-Family Residential (SMFR): Small-scale multi-family dwellings include duplexes, triplexes, fourplexes, motor court clusters, and row townhomes with density up to 20 dwelling units per acre.
5	High Density Residential (HDR)	Multi-Family (MFR): Multi-family dwellings including apartments, townhomes, condominiums, and other attached product types with a density range up to 24 units per acre.
6	School (S)	Elementary school (K-5) uses. In the event this Planning Area is not developed with an elementary school, it may be developed with LMDR or MDR dwelling units as described above.
7A, 8A, and 8B	Open Space Recreation (OS-R)	Recreational uses including parks, trails, athletic fields, passive parks and open spaces, greenbelts, and drainage facilities to be used as recreation facilities.
7B	Open Space Conservation (OS-C)	Passive recreational uses including trails and conserved open spaces.
9	-Public Facility - Civic Node (PF)	Civic uses such as a fire station, public schools, government facility, transit node, educational center, or other public or quasi-public use.
10, 11	Business Park (BP)	Light industrial, and similarly related uses including warehousing and distribution, assembly and light manufacturing, parcel delivery facilities, repair facilities, and business parks, including but not limited to corporate offices. Employee-intensive uses, including research and development, technology centers, "clean" industry, and supporting retail and point of sale uses are also permitted.
12	Commercial-Business Park (C-BP)	All uses permitted in the Business Park (BP) category and Commercial (C) category, except agri-commercial uses.
13	Commercial (C)	Allows neighborhood, local, and regional serving retail, service, and agri-commercial uses. The permissible development density is between the minimum and maximum floor area ratio (FAR) of 0.15 - 0.40. Hotel uses are also permitted in this category.

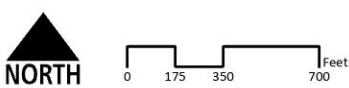
Notes:

1. General Descriptions only. Refer to Table 4H for a complete list of permitted uses.



Source(s): ESRI, RCTLMA (2023), Nearmap Imagery (2023), The Galloway Group (12-10-2021)

Figure 4-1



4.5 General Land Use Standards

General land use development standards for the MVSP include the following:

- 1) All utility lines shall be underground, except for major electrical lines (33kV or larger).
- 2) All signage shall be subject to City of Menifee Development Code Chapters 9.65 (Master Sign Program), 9.90 (Sign Permit), and 9.220 (Sign Regulations). Pertaining to Code Chapter 9.220 (Sign Regulations), the Residential and Open Space-Recreation planning areas shall be subject to the sign standards of Residential zoning districts, the Business Park and Commercial Business Park planning areas shall be subject to the sign standards of Industrial zoning districts and the Commercial planning area shall be subject to sign standards of Commercial and Office zoning districts. Any deviations may be permitted through City of Menifee review and approval of a Master Sign Program pursuant to Code Chapter 9.65 and which the City finds consistent with this Specific Plan's design guidelines for signage found in Specific Plan Subsection 5.4, Project Monumentation. Monument signs located at the main residential entry road connecting with Briggs Road and monument signs located along Briggs Road and Highway 74 to identify the name of the Business Park and Commercial Business Park are expressly permitted to be up to 48 square feet in size, but can be larger if approved as part of a Master Sign Program.
- 3) All waste storage areas shall comply with the City of Menifee Development Code Chapter 9.225, Waste Storage Facilities.

4.6 Planning Areas 1, 2, 3, 4, 5, and 6

Development Standards

R

The standards provided for in Table 4B establish development criteria for the residential planning areas within the Menifee Valley Specific Plan.

TABLE 4B - Planning Areas 1, 2, 3, 4, 5, and 6 Development Standards (R) ¹				
	LMDR	MDR	MHDR	HDR
Allowed Locations by Planning Area ²	1, 2, 3, 4, and 6		1, 3, and 5	5
Lot Dimensions (Minimum)				
Net Lot Area	6,000 sq.ft.	4,500 sq.ft.	2,300 sq.ft.	3,000 sq.ft.
Lot Width	40'	30'	30'	30'
Lot Depth	80'	80'	80'	80'
Frontage	40'	30'	30'	30'
Frontage - Flag Lot	20'	20'	20'	20'
Frontage - Knuckle or Cul-de-Sac	35'	30'	30'	35'
Setbacks (Minimum) ^{2, 3, 4, 5}				
Front Yard ^{4, 5, 6}	15'	15' 10'	10'	12' 10'
Corner Side Yard (measured from right of way)	10'	10'	10'	12' 10'
Corner Side Yard (measured from lot line)	5'	5'	5'	5'
Interior Side Yard	5'	4' min. 8' min. combined both sides	5' min. 10' min. combined both sides	5' min. 15' min. combined both sides
Corner Side Yard	10'	10'	10'	12'
Interior Side Yard	5'	5' min. 10' combined	5' min, 10' combined	5' min. 10' combined
Rear Yard (Living) ⁷	20'	10'	10'	15'
Rear Yard (Garage) ⁸	3'	3'	3'	3'
Building Height (Maximum) ^{7,9}				
Building Height	40'	40'	50'	50'
Wall and Fence Height (Maximum) ^{8, 9 10, 11, 12}				
Wall and Fence Height	6'	6'	6'	8'
Building Coverage (Maximum)				
Building Coverage	50%	50%	65%	65%
Open Space Coverage (Minimum)				
Open Space Req.	30%	30%	25%	20%
Private Open Space/Unit	N/A	100 sq.ft.	75 sq.ft.	75 sq.ft.
Notes:				
1. The development standards contained in Table 4B can be modified; as presented in Section 7, <i>Administration and Implementation</i> .				
2. Planning Area 6 is designated for School (S) land uses. In the event Planning Area 6 is not needed as a School site, Planning Area 6 may be developed with Residential land uses in accordance with Table 4B.				
3. Measured to property line, unless otherwise noted.				
4. Minimum setback of 5' between primary and all other detached structures on same lot.				
5. Setback encroachment allowances for architectural features and accessory structures can be found in Chapter 9.160.030 of the City of Menifee Development Code.				

6. Garages with entrances facing the street shall be set back no less than 20 feet.
7. Rear yard (living) setback may be reduced to 5' in cluster product types with minimum private open space.
8. Rear yard garage setback applies to garages oriented onto private drives.
9. Height of a residential structure shall be measured as the vertical distance from the average finished grade at the perimeter of the structure to the highest point of the structure.
10. Residential lots adjacent to Briggs Road are permitted a sound wall of up to a maximum height of 10 feet based on submission and approval of a noise study to the Community Development Director and City Engineer.
11. If all or any portion of a wall is used for retaining purposes, the maximum height shall be increased to 9 feet.
12. Corner side yard setbacks measured from building to street ROW.

Additional Residential Standards

- a) Residential development in the MVSP shall not exceed 1,718 residential dwelling units. During the site plan and tentative tract map stages of the development process, the final number of residential dwelling units for a particular planning area will be determined, which may be more or less than the target number of residential units identified for that planning area in this Specific Plan. Refer to Section 7, *Administration and Implementation*, for information on the implementation of residential development in the Specific Plan area.
- b) Planning Area 6 is the allowed location of an approximately 12.0 net-acre K-5 school site, but a school is a permitted use in any residential planning area.
- c) Any Agri-Commercial use area that may be developed in Planning Areas 5 or 13 shall be a minimum of 3.0 net acres in size.
- d) The final selection of dwelling unit types (LMDR, MDR, MHDR, and HDR) to be developed in Residential planning areas will occur at the tentative tract map stage of the development process based on market demand and regional housing needs as determined by the project proponent, approved by the City of Menifee, and subject to the residential development standards set forth in this Specific Plan. Refer to Section 7, *Administration and Implementation*, for information on the implementation of residential development in the Specific Plan area.
- e) Residential buildings within Planning Areas 2 and 4 must be setback a minimum of 100 feet from the boundary of Planning Area 10. Landscaping, utilities, accessory structures, roadways, sidewalks, and trails are permitted in the setback area.
- f) Side entry garages are permitted on lots with lot widths of 55 feet or greater to ensure sufficient vehicular driveway space is provided.
- g) Exceptions to maximum building height:
 1. Architectural Features. Chimneys, cupolas, flagpoles, monuments, radio and other towers, gas storage holders, water tanks and similar structures and mechanical

appurtenances may be permitted in excess of building height limits by no more than 10 feet, provided, however, that the same may be safely erected and maintained at such height in view of the specific conditions and circumstances affecting the structure and adjacent properties.

2. Antenna, Noncommercial. One noncommercial antenna may be permitted up to 52.5 feet in height for each parcel. Antennas shall be set back from all property lines at least 1.25 times the height of the antenna. Height does not include the additional height of whip antennas. Additional antennas shall require a conditional use permit.
 3. Windmills, Noncommercial. Noncommercial windmills are only permitted within the Agri-Commercial area.
- h) Tandem parking spaces are permitted throughout the residential planning areas.
 - i) Tractor trailers are prohibited from parking on residential streets.
 - j) Setback exceptions shall comply with the City of Menifee Development Code Chapter 9.160, Setbacks and Exceptions.
 - k) Lighting shall comply with the City of Menifee Development Code Chapter 9.205, Lighting Standards, for Light Zone 2.
 - l) Accessory structures shall comply with the City of Menifee Development Code Chapter 9.165, Accessory Structures.
 - m) All home-based businesses shall comply with the City of Menifee Development Code Chapter 9.255, Home Occupation and Home-Based Businesses.
 - n) Parking in the residential planning areas shall be as follows:

Minimum Parking Spaces	
Use	Per Unit
Single Family	
Single-Family	2.0 spaces
Duplex (two-family dwelling)	2.0 spaces
Attached/Multi-Family	
Multiple-family: single bedroom or studio	1.25 spaces
Multiple-family: two bedrooms	1.5 spaces
Multiple-family: three or more bedrooms	2.5 spaces
Senior citizen development	1.25 spaces
Leasing Office Employee	1 space per employee
Guest Parking	1 guest space per 10 units

Agri-Commercial Use Activity Development and Operational Standards

Agri-Commercial activity is permitted in Residential Planning Area 5 and/or Commercial Planning Area 13, at a minimum area of 3.0 net acres. Development and operational standards for this activity are as follows:

- a) Agri-Commercial use will consist of farming activity focused on crops and animal husbandry operations intended for distribution or sale on site.
- b) Hours of operations shall be limited between 6:00AM and 10:00PM.
- c) Operational measures shall be taken to minimize dust, noise and odor impacts upon surrounding properties.
- d) Animal husbandry operations shall have a minimum setback distance of 75 feet from any existing or planned residential lot line.
- e) Commercial and non-commercial animal husbandry activities for small, medium, and large animals shall be limited to Planning Area 13, and the Agri-Commercial area within Planning Area 5, unless otherwise allowed by the City of Menifee. Crowing fowl are not permitted in any Planning Area. Refer to Figure 2-2, *Conceptual Illustrative Site Plan*, for the conceptual location of the Agri-Commercial area within Planning Area 5. Refer to Table 4H, *Allowable Uses and Approval Requirements*, for more detail.
- f) Commercial activity associated with the Agri-Commercial operations shall be limited to the following:
 - Sale of produce, meat, and farming related products originating from the Agri-Commercial site or surrounding region.
 - Small scale commercial activity, such as "farm to table" restaurants, produce stands, nurseries, retail sales, services, and events relevant and ancillary to the Agri-Commercial activities.
- g) Onsite production of paintings, jewelry, leather goods, pottery, and similar items and sale of these items originating from the Agri-Commercial site or surrounding region.
- h) Non-retail uses such as greenhouse, crop and tree farming and wholesale nursery.
- i) Park and recreational uses that are passive in nature, such as picnicking, arts and crafts workshops, outdoor cultural performances.
- j) Administrative offices, employee meeting rooms, restrooms, and storage.
- k) All fences that enclose livestock shall be designed, constructed and maintained so as to control and contain such livestock at all times and so as to prevent such livestock from reaching across any property lines and damaging adjacent property.

- l) Educational, demonstration, and seasonal farm activities shall be permitted.
- m) Lighting associated with the Agri-Commercial activities shall comply with all applicable requirements set forth in the City of Menifee Development Code.

4.7 Planning Areas 10 and 11 Development Standards

BP

The following standards represented in Table 4C establish development criteria for Planning Areas 10 and 11 within the Menifee Valley Specific Plan.

TABLE 4C - Planning Areas 10 and 11 Development Standards	
Lot Configuration and Coverage	
Minimum Net Lot Area	5.0 acres
Maximum Building Coverage	50%
Minimum Floor Area Ratio (FAR)	none
Maximum Floor Area Ratio (FAR)	0.50 FAR average for all combined BP areas; 0.60 FAR maximum on any single lot
Maximum Height	
Building Height ¹	60'
Wall or Fence Height ^{2,3}	14'
Minimum Building Separation ⁴	30'
Minimum Building Setbacks	
Arterial (Menifee Road) ⁵	200'
Other Public Streets ⁵	40'
Residential Property Line	50' to a building face with no loading docks 250' to a building face with loading docks
Residential Structures ⁶	100' from Business Park Planning Area property line
School Property Line	100' to a building face with no loading docks (loading docks are not permitted to face a school)
All other lot lines	20'
Minimum Parking and Drive Aisle Setbacks⁷	
Arterial (Menifee Road) ⁵	50'
Other Public Streets	20'
Residential Property Line	20'
School Property Line	20'
Other Lot Lines	0'
Landscape Coverage⁸	
Landscape Coverage	12%
Notes:	
1. Building height shall be measured from the finished floor of the structure to the tallest building element. In case of sloped finished floor, this measurement shall be from finished floor to tallest building element directly above any finished floor elevation.	
2. Wall and fence height shall be measured from the finished grade of where the wall or fence meets the ground surface to the highest point of the element. Where additional height is needed based on security, noise attenuation needs, earthwork retaining, or specific site operating requirements, additional wall/fence height may be approved by the City's Community Development Director, subject to evaluation of agencies and necessity.	
3. For retaining walls that have different wall heights on opposite sides of the wall, the maximum wall height shall be measured using the outside (shorter) side of the wall and not the inside (taller) face of the wall, so long as the taller face of the wall faces interior to a Business Park lot and does face a public view.	
4. Ancillary structures, such as trash enclosures, are not subject to the building separation requirements.	

5. As measured from edge of right of way or access easement.
6. Landscaping, utilities (including wells and pump houses), accessory structures, roadways, sidewalks, and trails are permitted in the setback area.
7. Does not apply to access drives that need to cross perpendicular through the setback to connect with a public or private street.
8. Landscape coverage in Planning Areas 10 and 11 is based on a total aggregate basis.

Additional Planning Areas 10 and 11 Development Standards

- a) Development in MVSP Planning Areas 10 and 11 shall not exceed 4,360,000 square feet of building space. During the site plan and tentative tract map stages of the development process, the final amount of building square footage for a particular planning area will be determined, which may be more less than the target amount of square footage specified for that planning area in this Specific Plan. Refer to Section 7, *Administration and Implementation*, for information about the implementation of business park development in Planning Areas 10 and 11.
- b) Loading docks, truck parking areas, truck trailer storage areas, and shipping container storage areas shall be visually screened from view from public streets and adjacent residential uses, schools, and parks.
- c) Outdoor storage of materials and equipment shall be permitted ancillary to the land uses allowed per Table 4C. Outdoor storage areas shall be screened from view from public streets and adjacent residential uses, schools, and parks.
- d) Ground and roof-mounted exterior mechanical equipment, heating and ventilating, air conditioning, tanks, and other mechanical devices shall be of an architecturally compatible design with the primary structure and screened when visible to the public, except when screening interferes with fire and life safety requirements.
- e) The following encroachments are permitted in required yards.
 1. Sidewalks, pathways, and pedestrian or employee amenities such as benches.
 2. Signs.
 3. Light fixtures.
 4. Detention basins and water quality treatment facilities, provided they are integral to the landscape design.
 5. Utility-owned facilities if also located in an approved utility easement.
 6. Public art.
- f) The following encroachments are expressly not permitted in required yards unless screening is provided.
 1. Free-standing mechanical equipment associated with the operation of a permitted or conditionally permitted use such as HVAC equipment and generators.
 2. Outdoor display or storage of goods.
 3. Truck and trailer storage and truck courts.

- g) The following standards shall apply to the storage of truck trailers and shipping containers on any lot.
 - 1. Containers shall not be removed from trailer chassis and shall not be placed directly on the ground.
 - 2. Containers shall not be vertically stacked.
 - 3. Truck trailers and containers shall not be stored within two (2) feet of any required screening wall nor within twenty (20) feet of any structure or building, except where adjacent to or abutting loading dock doors.
 - 4. All truck trailers and containers shall be arranged in parallel rows so as to appear as an organized placement.
- h) All manufacturing and processing activities shall be conducted within a wholly enclosed building.
- i) No truck court accommodating diesel-fueled trucks or equipment shall operate within 250 feet of a residential property line or within 100 feet of a school property line, measured from the edge of pavement where a diesel truck or equipment could park or operate to the residential or school property line.
- j) If a truck must pass through a security gate to access a building, the entry gate shall be positioned at least 150 feet inside the property line. A minimum of one queuing space is required for any gated entry access and at least one space shall be provided per every 250,000 s.f. of building space or portion thereof. A queuing space shall measure, at a minimum, 8 feet by 70 feet. Spaces can be either side by side or single file placement.
- k) In areas where trucks park or load, the owner or operator shall provide on-site signage stating the anti-idling restrictions required by the California Air Resources Board (CARB).
- l) Lighting shall comply with the City of Menifee Development Code Chapter 9.205, Lighting Standards, for Light Zone 3.
- m) Parking for fulfillment, e-commerce, general warehousing, distribution and parcel delivery operations shall be as follows:

Minimum Parking Spaces
1 space per 1,000 s.f. for first 20,000 s.f. of building area
1 space per 2,000 s.f. for 20,000 - 100,000 s.f. of building area
1 space per 3,000 s.f. for building area over 100,000 s.f.

No additional parking shall be required for incidental office use associated with these building use types. All other parking and loading areas shall be subject to the City of Menifee Development Code Chapter 9.215, Parking and Loading Standards.

4.8 Planning Area 12 Development Standards

Development criteria for Planning Area 12 within the Menifee Valley Specific Plan, will be established per corresponding land use.

C-BP

The development standards specified in Section 4.7, *Planning Areas 10 and 11 Development Standards*, will apply to portions of Planning Area 12 developed with Business Park uses. Development standards specified in Section 4.9, *Planning Area 13 Development Standards*, will apply to portions of Planning Area 12 developed with Commercial uses.

Additional Planning Area 12 Development Standards

- a) Development in Planning Area 12 shall not exceed 1,150,000 square feet of building space. During the site plan and tentative tract map stages of the development process, the final amount of building square footage for a particular planning area will be determined, which may be more or less than the target amount of square footage specified for that planning area in this Specific Plan. Refer to Section 7, *Administration and Implementation*, for information on about the implementation of commercial business park development in Planning Area 12.
- b) All of the Business Park Development Standards and Additional Business Park Development Standards specified in Section 4.7 apply to portions of Planning Area 12 developed with Business Park uses.
- c) All of the Commercial Development Standards and Additional Commercial Development Standards specified in Section 4.9 apply to portions of Planning Area 12 developed with Commercial uses.
- d) Parking and loading areas shall be subject to the City of Menifee Development Code Chapter 9.215, Parking and Loading Standards

4.9 Planning Area 13 Development Standards

The following standards represented in Table 4D establish development criteria for Planning Area 13 within the Menifee Valley Specific Plan.

C

TABLE 4D - Planning Area 13 Development Standards	
Lot Configuration and Coverage	
Minimum Net Site Area	5.0 acres
Minimum Net Site Area for Development on Separate Lots	20,000 s.f.
Maximum Building Coverage	40%
Minimum Floor Area Ratio	0.15 FAR
Maximum Floor Area Ratio	0.30 FAR average for all combined C areas; maximum of 0.40 FAR on any single lot
Maximum Height	
Building Height	50'
Wall or Fence Height ¹	14'
Building Setbacks	
Highway 74 ²	50'
Other Public Streets ³	15'
School Property Line	60' to a building face with no loading docks 500' to a building face with loading docks
All other lot lines	10'
Parking and Drive Aisle Setbacks⁴	
Highway 74 ²	20'
Other Public Streets ²	20'
School Property Line	20'
All other lot lines	10'
Building Separation	
Building Separation	10'
Landscape Coverage	
Landscape Coverage	20%
Notes:	
1. Wall and fence height shall be measured from the finished grade of the property to the highest point of the element. Where additional height is needed based on security, noise attenuation needs, or specific site operating requirements, additional wall/fence height may be approved by the City's Community Development Director, subject to evaluation of agencies and necessity. 2. As measured from edge of right of way or access easement. 3. Lot size shall be of sufficient size to accommodate the proposed land use and meet all applicable development standards imposed by the Specific Plan 4. Does not apply to access drives that need to cross perpendicular through the setback to connect with a public or private street.	

Additional Planning Area 13 Development Standards

- a) Development in Planning Area 13 shall not exceed 560,000 square feet of building space.

- b) All of the Agri-Commercial Use Activity Development and Operational Standards given in Section 4.6 apply to any portion of Planning Area 13 developed with agri-commercial uses.
- c) Parking and loading areas shall be subject to the City of Menifee Development Code Chapter 9.215, Parking and Loading Standards.

4.10 Planning Areas 7A, 7B, 8A, and 8B Development Standards

The following standards represented in Table 4E establish development criteria for Planning Areas 7A, 7B, 8A, and 8B within the Menifee Valley Specific Plan.

OS-R**OS-C**

TABLE 4E - Planning Areas 7A, 7B, 8A, and 8B Development Standards	
Lot Configuration and Coverage	
Minimum Net Lot Area	No minimum
Maximum Building Coverage	10%
Maximum Floor Area Ratio	0.10 FAR
Maximum Height	
Building Height ¹	35'
Wall or Fence Height ^{2, 3}	6'
Building Setbacks	
Street	50'
All other lot lines	25'
Notes:	
1. Building height shall be measured from the finished floor of the structure to the tallest building element.	
2. Wall and fence height shall be measured from the finished grade of the property to the highest point of the element. Height may be increased if required for security or noise, per approval of Community Development Director.	
3. Along the segment of Briggs Road that curves around Planning Area 7B, if a wall is needed for retaining purposes, the maximum wallheight shall be increased to 50 feet.	

Additional Recreation and Open Space Development Standards

- 1) Fences adjacent to open space and trail areas shall be constructed and maintained as open view fencing, unless otherwise approved by the City, and shall not be chain link.
- 2) Park spaces shall have public access directly with a public or private street.
- 3) Active use recreational facilities open to the public shall provide off-street parking as required by the City of Menifee.
- 4) Lighting shall comply with the City of Menifee Development Code Chapter 9.205, Lighting Standards, for Light Zone 1.

4.11 Planning Areas 6 and 9 Development Standards

The following standards represented in Table 4F establish development criteria for the school land uses in Planning Area 6 and public facility(civic node) land uses in Planning Area 9 within the Menifee Valley Specific Plan.

PF

TABLE 4F - Planning Areas 6 and 9 Development Standards	
Lot Configuration and Coverage	
Minimum Net Lot Area	1.0 acre
Maximum Building Coverage	35%
Maximum Height	
Building Height ¹	40'
Wall or Fence Height ²	6'
Minimum Building Setbacks	
Street	25'
All other lot lines	15'
Notes:	
1. Building height shall be measured from the finished floor of the structure to the tallest building element. 2. Wall and fence height shall be measured from the finished grade of the property to the highest point of the element. Height may be increased if required for security or noise, per approval of Community Development Director.	

Public Facility - Civic Node Development Standards

- 1) Development in Planning Area 9 shall not exceed 120,000 square feet of building space.
- 2) The acreage of Planning Area 9 may be modified as long as 4.0 net acres of usable area is maintained.
- 3) Development of a fire station site within Planning Area 9 shall adhere to the development standard requirements of the City of Menifee and the Riverside County Fire Department, of which the City contracts with for fire protection service.
- 4) Lighting in Planning Area 9 shall comply with the City of Menifee Development Code Chapter 9.205, Lighting Standards, for Light Zone 3.

School Site (K-5) Development Standards

A K-5 elementary school site is a permitted use in any Residential planning area but is allowed to be developed in Planning Area 6, at approximately 12.0 net acres in size. Development standards for this activity are as follows:

- a) The proposed school location and site design shall be established to minimize traffic impact upon adjacent and immediate vicinity residential uses.
- b) The school site shall be located a minimum distance of 500 feet from Business Park and Commercial-Business Park Planning Areas 10, 11, and 12.
- c) Off-street parking shall meet the minimum requirements of the Romoland School District.
- d) Student load/unload areas shall be designed to accommodate peak demand periods without interfering or blocking traffic on abutting streets.
- e) Site design and pedestrian improvements shall be established to encourage a walkable environment for students within the MVSP.
- f) Lighting shall comply with Romoland School District standards.
- g) In the event Planning Area 6 is not needed as a School site, Planning Area 6 may be developed with Residential land uses and would be subject to the development standards contained in Table 4B. All of the Residential Development Standards for LMDR and MDR and Additional Residential Development Standards given in Section 4.6 apply to any portion of Planning Area 6 developed with residential units.

4.13 Permitted Use Standards

The Menifee Valley Specific Plan identifies several permitted, conditional, and temporary uses within the various planning areas, as represented in Table 4H, which is provided at the end of this Section. A permitted use is a land use activity permitted by right in the area designated as such. A Permitted land use proposed upon vacant land shall be subject to the procedures identified in Menifee Municipal Code Title 9 (Planning and Zoning). A conditional use shall be subject to review for both the land use activity and the structure in which that activity shall be conducted. Temporary uses may be permitted for a period provided for in a permit in accordance with Menifee Development Code Chapter 9.105. Those uses not specifically listed in Table 4H are subject to a determination by the City's Community Development Director.

Table 4H Allowable Uses and Approvals															
Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	Additional Permits and Requirements (Refer to City of Menifee Development Code)
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
RESIDENTIAL USES															
Accessory Dwelling Unit (ADU)	P	P	P	P	P	P	--	--	--	--	--	--	--	--	Chapter 9.35 (Accessory Dwelling Unit Permit)
Bed & Breakfast Establishment	C	C	C	C	C	C	--	--	--	--	--	--	--	--	Chapter 9.245 (Bed and Breakfast)
Duplex (Two-Family Dwelling)	P	P	P	P	P	P	--	--	--	--	--	--	--	--	
Family Day Care Home, Small	P	P	P	P	P	P	--	--	--	--	--	--	--	--	
Family Day Care Home, Large	P	P	P	P	P	P	--	--	--	--	--	--	--	--	Chapter 9.60 (Large Family Day Care Permit)
Group Residential Facility - Six or Fewer Residents	P	P	P	P	P	P	--	--	--	--	--	--	--	--	
Group Residential Facility - Seven or More Residents	C	C	C	C	C	C	--	--	--	--	--	--	--	--	

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	Additional Permits and Requirements (Refer to City of Menifee Development Code)
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Guest House, Accessory to Single-Family Detached	P	P	--	--	--	--	--	--	--	--	--	--	--	--	Chapter 9.295 (Special Housing Types)
Home Occupation	P	P	P	P	P	P	--	--	--	--	--	--	--	--	Chapter 9.255 (Home Occupation and Home-Based Business)
Live/Work units	C	C	C	C	C	C	--	--	--	--	--	--	C	C	
Multiple Family	C	C	P	P	P	P	--	--	--	--	--	--	--	--	
Residential Care Facility	C	C	C	C	C	C	--	--	--	--	--	--	--	--	Chapter 9.270 (Community Care Facilities)
Single-Family Detached	P	P	P	C	C	C	--	--	--	--	--	--	--	--	
Supportive Housing	C	C	C	C	C	C	--	--	--	--	--	--	--	--	
Transitional Housing	C	C	C	C	C	C	--	--	--	--	--	--	--	--	
OPEN SPACE/CONSERVATION USES															
Athletic Field, Clubhouse, and Community Center	P	P	P	P	P	P	P	--	--	--	--	--	P	--	
Aquatic Center	P*	P*	P*	P*	P*	P*	P*	--	--	--	--	--	P*	--	*Only allowed in public or private park
Community Center	C	C	C	C	C	C	--	--	--	P	--	--	P	P	
Community Garden and Nursery	P	P	P	P	P	P	P	--	P	--	--	--	--	--	
Conservation Lands, Natural and Scenic Resources Protection	P	P	P	P	P	P	P	P	P	P	P	P	P	P	
Nature and Visitor Center	C	C	C	C	C	C	P	P	P	P	--	--	--	--	
Parks and Recreation Areas	P	P	P	P	P	P	P	--	P	P	--	--	P	--	

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	Additional Permits and Requirements (Refer to City of Menifee Development Code)	
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13		
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use																
NON-RESIDENTIAL, COMMERCIAL, INDUSTRIAL, AND PUBLIC/QUASI PUBLIC USES																
Ambulance Services	--	--	--	--	--	--	--	--	--	P	C	C	C	C		
Amusement and Game Arcade	--	--	--	--	--	--	--	--	--	--	C	C	C	P		
Amusement Park (Including Multiple Activities Such as Simulated Flying, Racing, Mini-Golf, etc.):																
Indoor	--	--	--	--	--	--	--	--	--	--	C	C	P	P		
Outdoor	--	--	--	--	--	--	--	--	--	--	C	C	C	C		
Animal Hospitals and Veterinary Services	--	--	--	--	--	--	--	--	--	--	C	C	P	P	Outside collection bins prohibited	
Antique Shops and Thrift Stores	--	--	--	--	--	--	--	--	--	--	C	C	C	P		
Art Gallery, Art Studio, Library, Reading Room, Museum	--	--	--	--	--	--	--	--	--	P	--	--	P	P		
Art Supply Shops and Studios	--	--	--	--	--	--	--	--	--	--	--	--	P	P		
Auction Houses:																
Indoor	--	--	--	--	--	--	--	--	--	--	P	P	P	P		
Auditoriums, Event Centers, and Assembly Areas, including Live Entertainment:																
Indoor	--	--	--	--	--	--	--	--	--	C	C	C	P	--	Chapter 9.250 (Alcohol Sales)	
Outdoor	--	--	--	--	--	--	--	--	--	C	--	--	--	C		
Bakery Goods Distributors	--	--	--	--	--	--	--	--	--	--	C	C	C	P*	*Only west of the north-south Industrial Collector Road	
Bakery Shops, Coffeehouse Cybercafe, Delicatessens, Ice Cream Shops	--	--	--	--	P*	--	--	--	--	--	C	C	P	P	*Only per MVSP Section 4.6 - Agri-Commercial allowance	
Banks and Financial Institutions	--	--	--	--	--	--	--	--	--	--	--	--	P	P		
Banquet Facilities	--	--	--	--	--	--	--	--	--	--	C	C	C	C		
Batting Cages: Indoor	--	--	--	--	--	--	--	--	--	--	P	P	C	P		

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	Additional Permits and Requirements (Refer to City of Menifee Development Code)
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Batting Cages: Outdoor	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Billiard and Pool Halls, Bowling Alleys	--	--	--	--	--	--	--	--	--	--	--	--	P	P	Chapter 9.250 (Alcohol Sales)
Breweries, Distilleries, and Wine Making Facilities with On-Site Tasting Room and Sales for Off-Site Consumption	--	--	--	--	--	--	--	--	--	--	P	P	C*	P	Chapter 9.250 (Alcohol Sales) *Sales for off-site consumption require a Conditional Use Permit.
Business Services	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Car Washes: Commercial	--	--	--	--	--	--	--	--	--	--	C	C	C	C	
Car Washes: Non-Commercial and Incidental to a Primary Use	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Catering Services	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Churches, Synagogues, Temples, and Other Religious Facilities*	C	C	C	C	C	C	--	--	--	C	C	C	C	C	*Only when directly accessible off a General Plan roadway
Clinics, Including, but not Limited to Medical and Urgent Care	--	--	--	--	--	--	--	--	--	--	--	--	C	P	
College or University	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Commercial Recreational Facilities (Not associated with Open Space Recreational uses.) (Includes Commercial Archery Ranges, Athletic Playgrounds, Motor Driven Vehicles and Riding Academies, Sports Arenas, Sports Courts, Skating Rinks, Skate Parks Stadiums, and Swimming Pools):															
Indoor	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Outdoor	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Commercial Television and Radio Broadcast Structures	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Congregate Care Facility	--	--	--	--	C	--	--	--	--	--	--	--	--	--	Chapter 9.270 (Community Care Facilities)

Table 4H
Allowable Uses and Approvals

Table 4H Allowable Uses and Approvals															
Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	Additional Permits and Requirements (Refer to City of Menifee Development Code)
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Convalescent Hospital/Care Facility	--	--	--	--	--	--	--	--	--	--	--	--	C	---	
Convenience Stores, Not Including the Sale of Motor Vehicle Fuel	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Dance Halls, Night Clubs, Discos, Cabarets, Cocktail Lounges, Lodges and Incidental Dancing Areas, and Similar Facilities Where Dancing is the Principal Use	--	--	--	--	--	--	--	--	--	--	--	--	C	C	Chapter 9.250 (Alcohol Sales)
Data Center	--	--	--	--	--	--	--	--	--	--	C	C	C	--	
Day Care Center	--	--	--	--	--	--	--	--	--	--	--	--	C	P	Chapter 9.270 (Community Care Facilities)
Drug Store	--	--	--	--	--	--	--	--	--	--	--	--	P	P	Chapter 9.250 (Alcohol Sales)
Dry Cleaning and Laundromat (Except Uniform Supply and Industrial Launderers);	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Educational Institution, other than a Public Elementary School	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Educational Institution, Public Elementary School	--	--	--	--	--	P	--	--	--	--	--	--	--	--	
Emergency Shelter (1)	--	--	--	--	--	--	--	--	--	--	P	P	P	P	As mandated by State Law and City Housing Element. See Footnote 1 Below.

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	Additional Permits and Requirements (Refer to City of Menifee Development Code)
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Equipment (New) Sales Small (Including Rototillers, Power Mowers, Sanders, Power Saws, Cement and Plaster Mixers Not Exceeding 20 Cubic Feet in Capacity and Other Similar Equipment)	--	--	--	--	--	--	--	--	--	--	P	P	C	P	Within an Enclosed Building
Equipment (New) Sales, Large (Including Large Vehicles, Trucks with Beds Over 18 Feet in Length, Eighteen Plus (18+) Wheelers, and Construction Equipment)	--	--	--	--	--	--	--	--	--	--	--	C	--	--	Within an Enclosed Building
Farms and Agricultural Operations:															
Commercial	--	--	--	--	P*	--	--	--	--	--	--	--	--	P*	*Only per MVSP Section 4.6 - Agri-Commercial allowance. Animal Keeping of small, medium, and large animals permitted in accordance with Chapter 9.235 (Animal Keeping). Ancillary retail sales permitted on-site.
Fast Food/Quick Service, with Drive-Through	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Fast Food/Quick Service, without Drive-Through	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Feed and Grain Sales	--	--	--	--	C*	--	--	--	--	--	C	C	C	C	*Only per MVSP Section 4.6 - Agri-Commercial allowance
Fire or EMS Facility	--	--	--	--	--	--	--	--	--	P	C	C	C	C	

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	Additional Permits and Requirements (Refer to City of Menifee Development Code)
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Fowl, Non-Crowing, Commercial	--	--	--	--	C*	--	--	--	--	--	--	--	C*	C*	
Fulfillment Centers and E-Commerce Facilities	--	--	--	--	--	--	--	--	--	--	P	P	P	--	
Future Farmers of America and 4-H Programs	--	--	--	--	P*	--	--	--	--	--	--	--	P*	C*	Chapter 9.235 (Animal Keeping) *Only per MVSP Section 4.6 - Agri-Commercial allowance
Gas Station with/without Convenience Store, with/without Alcoholic Beverage Sales, and with/without Car Wash	--	--	--	--	--	--	--	--	--	--	--	--	C*	C	*Only when adequate buffer is provided with residential uses per the City of Menifee Development Code
Gas Station, Non-Commercial and Incidental to a Primary Use	--	--	--	--	--	--	--	--	--	--	C	C	--	--	
General Retail	--	--	--	--	C*	--	--	--	--	--	--	--	P	P	
General Warehousing, Distribution Centers, and Storage (Except Noxious, Explosives, or Dangerous Materials)	--	--	--	--	--	--	--	--	--	--	P	P	P	--	
Government Facility (Library, etc.) or Office	--	--	--	--	C	--	--	--	--	P	P	P	P	P	
Government Maintenance, Storage, or Distribution Facility	--	--	--	--	--	--	--	--	--	P	P	P	--	--	
Grocery Store	--	--	--	--	--	--	--	--	--	--	--	--	P	P	Chapter 9.250 (Alcohol Sales)
Guns and Ammunition Store	--	--	--	--	--	--	--	--	--	--	--	--	--	C	

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	Additional Permits and Requirements (Refer to City of Menifee Development Code)
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Hardware or Building Supplies Sales:															
Without Outdoor Sales/Storage	--	--	--	--	--	--	--	--	--	--	P	P	P	P	Outdoor Areas Must be Screened
With Outdoor Sales/Storage	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Health, Fitness, Dance, Martial Arts Studio:															
Non-Commercial, Only When Incidental to a Primary Use	--	--	--	--	C	--	--	--	--	--	P	P	P	P	
Commercial, <5,000 sq. ft.	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Commercial, >5,000 sq. ft.	--	--	--	--	--	--	--	--	--	--	C	C	C	C	
Hospitals	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Hotels and Resort Hotels	--	--	--	--	--	--	--	--	--	--	--	--	C	P	
Kennel or Cattery	--	--	--	--	--	--	--	--	--	--	C	C	C	C	Chapter 9.235 (Animal Keeping)
Laboratories, Research and Development	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Outdoor Storage	--	--	--	--	--	--	--	--	--	--	P	P	--	--	Permitted in Planning Areas 10 and 11, if completely screened.
Manufacturing, Handcraft	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Manufacturing, Light-Intensity	--	--	--	--	--	--	--	--	--	--	P	P	C	--	
Manufacturing, Medium-Intensity	--	--	--	--	--	--	--	--	--	--	C	C	--	--	Refer to Section 4.7 of the MVSP for definitions.
Massage Establishment	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Material Storage Yard:															
Primary Use	--	--	--	--	--	--	--	--	--	--	C	C	--	--	

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	Additional Permits and Requirements (Refer to City of Menifee Development Code)
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Only When Incidental to a Primary Use	--	--	--	--	--	--	--	--	--	--	P	P	C	C	*Only permitted as ancillary to a primary land use in this Table. Permitted in Planning Areas 10 and 11, if completely screened. CUP required if not fully screened.
Membership Clubs, Organizations, and Lounges	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Motels	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Motor Vehicle Body, Paint, and Upholstery Shops:															
Primary Use	--	--	--	--	--	--	--	--	--	--	C	C	C	C*	Chapter 9.240 (Motor Vehicle and Related Uses) *Only west of the north-south Industrial Collector Road
Only When Incidental to a Primary Use	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Motor Vehicle Parts and Supply Store	--	--	--	--	--	--	--	--	--	--	--	--	C	P	
Motor Vehicle Repair/Services (e.g. Tune-Ups, Emission Test, Brakes, Tires, Batteries, Electrical.):															
Primary Use	--	--	--	--	--	--	--	--	--	--	C	C	C	C*	Chapter 9.240 (Motor Vehicle and Related Uses) *Only west of the north-south Industrial Collector Road
Only When Incidental to a Primary Use	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Motor vehicle sales and rental (including outdoor display area and repairs associated with sales) - Automobile Sales, Automobile Rental, Boats, Recreational Vehicle, Trailer and Mobile Home Sales and Rental:															
Primary Use	--	--	--	--	--	--	--	--	--	--	C	C	C	C	Chapter 9.240 (Motor Vehicle and Related Uses)
Only When Incidental to a Primary Use	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Motor Vehicle Wholesale, With No Outdoor Display	--	--	--	--	--	--	--	--	--	--	P	P	P	P	Chapter 9.240 (Motor Vehicle and Related Uses)

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	Additional Permits and Requirements (Refer to City of Menifee Development Code)
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Motor Vehicle or Trailer Storage: Indoor	--	--	--	--	--	--	--	--	--	--	P	P	P	P	Chapter 9.240 (Motor Vehicle and Related Uses)
Motor Vehicle or Trailer Storage: Outdoor															
Primary Use	--	--	--	--	--	--	--	--	--	--	C	C	C	--	
Only When Incidental to a Primary Use	--	--	--	--	--	--	--	--	--	--	P*	P*	P*	C	Chapter 9.240 (Motor Vehicle and Related Uses) *Only permitted as ancillary to a primary land use in this Table. Permitted in Planning Areas 10, 11, and 12, if completely screened. CUP required in Planning Area 13.
Motorcycle Sales/Service	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Movie Theatre	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Natural Gas, Above Ground Storage, Only When Incidental to a Primary Use	--	--	--	--	--	--	--	--	--	--	P	P	C	C	Setback minimum of 500' from residential or school property line.
Nurseries and Garden Supply, Indoor and Outdoor (Retail Sales): Primary Use	--	--	--	--	--	--	--	--	--	--	--	--	P	P	Does not Include Cultivation of Marijuana/ Cannabis.
Offices (Professional and Medical)	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Parcel and Package Sorting and Distribution Centers	--	--	--	--	--	--	--	--	--	--	P	P	P	--	

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	Additional Permits and Requirements (Refer to City of Menifee Development Code)
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Park and Ride	--	--	--	--	--	--	--	--	--	P	P	P	P	P	
Parking Structures	--	--	--	--	--	--	--	--	--	P	P	P	P	P	
Personal and Professional Services	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Pet Shops, Pet Supply, and Pet Care and Grooming	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Police Station and Substation	--	--	--	--	--	--	--	--	--	P	P	P	P	P	
Post Office	--	--	--	--	--	--	--	--	--	P	C	C	C	C	
Public Utility Facilities	P	P	P	P	P	P	--	--	P	P	P	P	P	P	
Public Utility Offices and Storage Buildings	--	--	--	--	--	--	--	--	--	P	P	P	P	P	
Radio and Television Broadcasting and Recording Studios	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Recycling Collection Facilities	--	--	--	--	--	--	--	--	--	--	C	C	C	C	
Recycling Processing Facilities	--	--	--	--	--	--	--	--	--	--	C	C	C	--	
Restaurant	--	--	--	--	P*	--	--	--	--	--	--	--	P	P	Chapter 9.250 (Alcohol Sales) *Only per MVSP Section 4.6 - Agri-Commercial allowance
Restaurants with Breweries, Distilleries and/or Wine Making Facilities with Sales for On-Site Consumptions	--	--	--	--	--	--	--	--	--	--	--	--	P	P	Chapter 9.250 (Alcohol Sales)
Restaurants with Breweries, Distilleries	--	--	--	--	--	--	--	--	--	--	--	--	C	C	Chapter 9.250 (Alcohol Sales)

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	Additional Permits and Requirements (Refer to City of Menifee Development Code)
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
and/or Wine Making Facilities with Sales for Off-Site Consumption															
Self-Storage, Public Storage Facilities (New)	--	--	--	--	--	--	--	--	--	--	C	C	C	C	
Senior Center	--	--	--	--	--	--	--	--	--	P	--	--	--	--	
Shooting Range; Indoor	--	--	--	--	--	--	--	--	--	--	C	C	--	--	
Simulated Shooting Games, Indoor (Laser Tag, etc.)	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Smoking Lounge	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Solar Battery, Storage	--	--	--	--	--	--	--	--	--	--	P	P	P	C	Setback minimum of 500' from residential or school property line.
Specialized Retail	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Studios for professional work in or teaching of any form of fine arts	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Tattoo Establishments	--	--	--	--	--	--	--	--	--	--	--	--	C	C	Chapter 9.265 (Tattoo Establishments)
Tourist Information Centers	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Towing Services (With Tow Truck Parking - No Auto Storage)	--	--	--	--	--	--	--	--	--	--	C	C	--	--	
Transportation Stations (Bus, Railroad and Taxi)	--	--	--	--	--	--	--	--	--	P	P	P	P	P	
Trucking, Draying, and Freightage Operations, including Truck Terminals with Outdoor	--	--	--	--	--	--	--	--	--	--	P*	P*	P*	--	*Only permitted as ancillary to a primary land use in this Table. Outdoor trailer storage must be screened.

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	Additional Permits and Requirements (Refer to City of Menifee Development Code)
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
Trailer Storage Only When Incidental to a Primary Use															
Uniform Supplies and Industrial Launderers	--	--	--	--	--	--	--	--	--	--	C	C	--	--	
Vehicle Storage and Impoundment within an Enclosed Building Only When Incidental to a Primary Use	--	--	--	--	--	--	--	--	--	--	P*	P*	C*	C*	*Only permitted as an accessory use to a primary use and must be screened.
Vocational/Trade School	--	--	--	--	--	--	--	--	--	--	P	P	C	C	
Warehouse/Club Store, Standalone Facility 50,000 Square Feet or Larger	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Warehouse/Club Store, Standalone Facility Under 50,000 Square Feet	--	--	--	--	--	--	--	--	--	--	--	--	P	P	
Wedding Chapels	--	--	--	--	--	--	--	--	--	--	--	--	C	C	
Wholesale Businesses with Samples on the Premises - No Storage	--	--	--	--	--	--	--	--	--	--	P	P	P	P	
Wholesale Stores and Distributors	--	--	--	--	--	--	--	--	--	--	P	P	P	C*	*Only west of the north-south Industrial Collector Road
Wireless Communication Facilities	C	C	C	C	C	C	C	--	--	C	C	C	C	C	Chapter 9.290 (Wireless Communications Facilities)

Table 4H
Allowable Uses and Approvals

Land Use Designation	Residential					S	OS-R	OS-C	OS-R	PF	BP		C-BP	C	
Planning Area	1	2	3	4	5	6	7A	7B	8A 8B	9	10	11	12	13	Additional Permits and Requirements (Refer to City of Menifee Development Code)
P = Permitted Use; C = Conditional Use or Additional Permit Required; T = Temporary Use															
<p>NOTES:</p> <p>1. Emergency Shelters shall:</p> <p>A. Provide on-site facilities management personnel during all hours that the emergency shelter is in operation.</p> <p>B. Be located no less than 300 feet from another emergency shelter.</p> <p>C. Provide adequate lighting to illuminate the entire outdoor and parking areas of the property.</p> <p>D. Provide security during all hours that the emergency shelter is in operation.</p>															

Section 5

Landscape

Guidelines



Brookfield
Properties

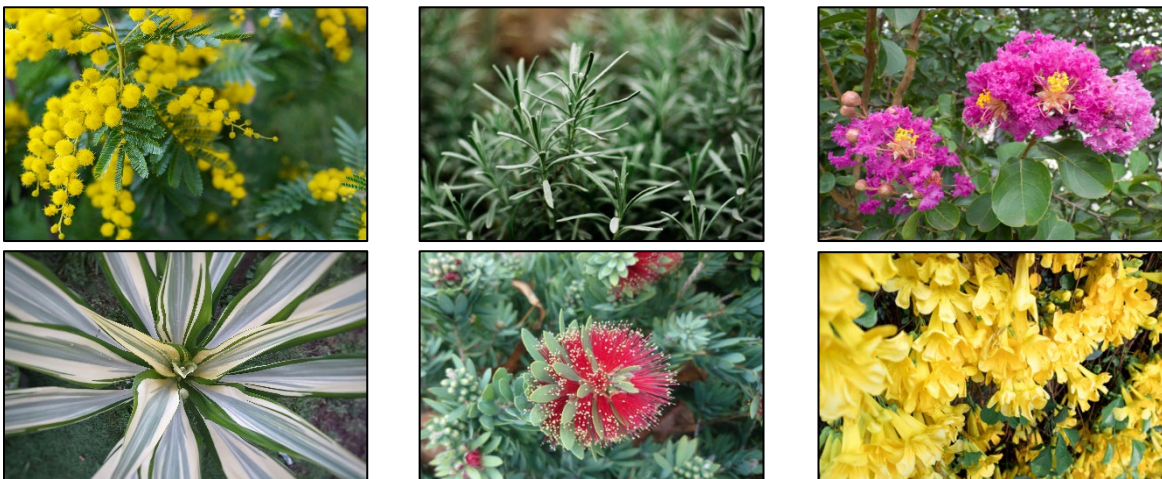
5 - Landscape Guidelines

The Landscape Guidelines contained in this section establish the landscape design principles that apply to all development in the MVSP area. The intent is to ensure that streetscapes, parks, recreation areas, greenbelts, community entries, and other common areas where landscape occurs are complementary in theme, plant material selection, and other landscape architectural design vocabulary. These Landscape Guidelines, when coupled with the companion Architecture Guidelines given in Section 6, establish an identity for Menifee Valley that is contextually appropriate, visually appealing, and thoughtfully planned.

The guidance for landscape design presented herein is intended to be used for direction and inspiration during the creation of final landscape design plans, and is not intended to be a rigid requirement in every respect. Flexibility in application and interpretation is expected to allow for creativity in final landscape design, so long as the thematic concept described herein is captured to unify the landscape concept across the Specific Plan area.

5.1 Landscape Plant Palette

The plant palette for the Specific Plan is shown in Table 5A, *Menifee Valley Plant Palette*. The plant palette selected for Menifee Valley is colorful, drought tolerant, and complements the natural setting. The recommended plant species also are selected for their appropriateness to climatic conditions, soil conditions, and concern for maintenance and water conservation. Plants are to be selected and grouped together in the landscape according to their sun and shade preferences, soil type preferences, and watering needs. Predominantly low water use planting are to be used in the majority of landscaped areas. Water-efficient landscapes also typically require less pruning and fertilizer than conventional landscapes.



The landscape theme includes colorful blooms and drought tolerant species.

Table 5A - Meniffee Valley Plant Palette

(PF denotes the plant factor water use requirement as L = Low, M + Medium, H = High)

BOTANICAL NAME	COMMON NAME	PF	STREETSCAPES	ENTRIES INTERSECTIONS	PARKS PASEOS REC CENTERS	BASINS SWALES
TREES						
<i>Albizia julibrissin</i>	Mimosa Tree	L	•	•	•	
<i>Arbutus unedo</i>	Strawberry Tree	L	•	•	•	
<i>Brachychiton populneus</i>	Bottle Tree	L	•		•	
<i>Cercidium 'Desert Museum'</i>	Desert Museum Palo Verde	L	•	•	•	
<i>Chamaerops humilis</i>	Medium Fan Palm	L		•	•	
<i>Chilopsis linearis</i>	Desert Willow	L	•		•	•
<i>Geijera parviflora</i>	Australian Willow	M	•	•	•	•
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree	M	•		•	
<i>Lagerstroemia indica</i>	Crepe Myrtle	M	•	•	•	
<i>Olea europaea 'Swan Hill'</i>	Swan Hill Olive	L		•	•	
<i>Phoenix dactylifera</i>	Date Palm	M	•	•	•	
<i>Pinus eldarica</i>	Afghan Pine	L	•		•	
<i>Pistacia chinensis</i>	Chinese Pistache	M	•	•	•	
<i>Platanus acerifolia</i>	London Plane Tree	M	•		•	
<i>Platanus racemosa</i>	California Sycamore	M	•		•	•
<i>Quercus agrifolia</i>	Coast Live Oak	L	•	•	•	
<i>Quercus virginiana</i>	Southern Live Oak	M	•	•	•	
<i>Rhus lancea</i>	African Sumac	L	•		•	
<i>Ulmus parvifolia</i>	Chinese Evergreen Elm	L	•		•	
SHRUBS						
<i>Arbutus unedo 'compacta'</i>	Compact Strawberry Tree	M	•	•	•	
<i>Baccharis centennial</i>	Centennial Baccharis	L			•	•
<i>Callistemon 'Little John'</i>	Little John Bottlebrush	L	•	•	•	
<i>Cotoneaster species</i>	Cotoneaster	M	•		•	
<i>Dietes bicolor</i>	Fortnight Lily	M	•	•	•	
<i>Elaeagnus pungens</i>	Silver Berry	L	•		•	
<i>Feijoa sellowiana</i>	Pineapple Guava	M		•		
<i>Hesperaloe parviflora</i>	Red Yucca	L	•	•	•	
<i>Heteromeles arbutifolia</i>	Toyon	L			•	•

BOTANICAL NAME	COMMON NAME	PF	STREETSCAPES	ENTRIES INTERSECTIONS	PARKS PASEOS REC CENTERS	BASINS SWALES
<i>Ilex vomitoria</i> 'Stokes Dwarf'	Dwarf Yaupon Holly	L	•	•	•	
<i>Iva hayesiana</i> <i>Leucophyllum</i> 'Frutescens'	San Diego Marsh Elder	M				•
<i>Nandina domestica</i> 'Compacta'	Texas Ranger	L	•	•	•	
<i>Nerium oleander</i> 'Petite Hybrids'	Dwarf Heavenly Bamboo	M		•	•	
<i>Olea europaea</i> 'Little Ollie'	Dwarf Oleander	L	•		•	
<i>Pittosporum tobira</i> and 'Wheeler's Dwarf'	Little Ollie	L		•	•	
<i>Prunus ilicifolia</i> <i>Punica granatum</i> 'Nana'	Mock Orange	M		•	•	
<i>Pyracantha</i> sp.	Hollyleaf Cherry	L			•	
<i>Raphiolepis indica</i> <i>Rosmarinus officinalis</i>	Dwarf Pomegranate	L	•		•	
<i>Salvia leucantha</i> <i>Westringia fruticosa</i>	Firethorn	M	•		•	
<i>Xylosma congestum</i> 'Compacta'	Indian Hawthorn	M	•	•	•	
	Rosemary	L	•		•	
	Mexican Bush Sage	L	•		•	
	Coast Rosemary	L	•		•	
	Compact Xylosma	M	•	•	•	
GROUND COVER						
<i>Acacia redolens</i> 'Desert Carpet'	Dwarf Trailing Acacia	L	•		•	
<i>Baccharis pilularis</i> 'Pigeon Point'	Dwarf Coyote Brush	L	•		•	•
<i>Eriogonum fasciculatum</i> 'Warriner Lytle'	Warriner Lytle Buckwheat	L	•		•	
<i>Juniper wiltonii</i> <i>Myoporum parvifolium</i>	Blue Rug Juniper	M	•	•		
<i>Rosa</i> 'Flower Carpet'	Myoporum	L	•	•		
<i>Rosmarinus officinalis</i> 'Prostratus'	Flower Carpet Rose	M	•	•		
<i>Senecio mandraliscae</i>	Trailing Rosemary	L	•		•	
	Blue Chalk Sticks	L	•	•	•	
SUCCULENTS						
<i>Agave americana</i> 'Mediopicta Alba'	White-Striped Century Plant	L	•	•	•	
<i>Agave</i> 'Blue Glow'	Blue Glow Agave	L	•	•	•	
<i>Agave parryi</i>	Parry's Agave	L	•	•	•	

BOTANICAL NAME	COMMON NAME	PF	STREETSCAPES	ENTRIES INTERSECTIONS	PARKS PASEOS REC CENTERS	BASINS SWALES
<i>Aloe Blue Elf</i>	Blue Elf Aloe	L	•	•	•	
<i>Dasyllirion wheeleri</i>	Desert Spoon	L	•	•	•	
<i>Euphorbia rigida</i>	Silver Spurge	L	•	•	•	
<i>Senecio mandraliscae</i>	Blue Chalk Sticks	L	•	•	•	
<i>Yucca filamentosa</i>	Bright Edge Yucca	L	•	•	•	
<i>Yucca 'Bright Edge'</i>	Bright Star Yucca	L	•	•	•	
<i>Yucca 'Walbristar'</i>						
GRASSES						
<i>Bouteloua gracilis</i>	Blonde Ambition					
<i>'Blonde Ambition'</i>	Blue Gramma Grass	L	•		•	•
<i>Carex spissa</i>	San Diego Sedge	L				•
<i>Elymus triticoides</i>	Creeping Wild Rye	L				•
<i>Juncus patens</i>	Common Rush	L	•			•
<i>Leymus condensatus</i>						
<i>'Canyon Prince'</i>	Creeping Wild Rye	L		•		•
<i>Leymus triticoides</i>	Creeping Wild Rye	L				•
<i>Muhlenbergia capillaris</i>						
<i>'Regal Mist'</i>	Regal Mist Muhly	L	•		•	
<i>Muhlenbergia rigens</i>	Deer Grass	M	•		•	•
VINE						
<i>Distictis buccinatoria</i>	Blood Red Trumpet Vine	M		•		
<i>Macfadyena unguis-cati</i>	Cat's Claw Vine	L	•			
<i>Wisteria floribunda</i>	Wisteria	M		•		
TURF GRASS - SEED						
<i>Cynodon dactylon</i>	Hybrid Bermudagrass	H			•	
cv.						
Marathon II Sod	Dwarf Tall Rescue	H		•	•	

5.1.1 Drought Tolerance

Drought-tolerant plantings are encouraged throughout the community. Although a plant may be drought tolerant, that plant still requires proper care, installation, watering, and maintenance to maximize its drought tolerance capabilities. Drought-tolerant plants should be grouped together with proper irrigation settings to avoid over-watering.

1. *Degrees of Drought Tolerance:* There are degrees of drought tolerance with some plants able to withstand or go without water for a greater period of time than others.
2. *Plant Installation Water Demand:* Drought tolerant plants like other plants require more watering during the initial installation period and for at least a three-month maintenance period to become established. Therefore, if drought tolerant plants are installed in the warmer months, more supplemental water will be required until the plant is established.
3. *Deep Watering Practices:* Drought tolerant plants like most plants need the proper deep watering practices to encourage deep root system development. Drought tolerant plants with a shallow root system resulting from frequent light applications of water will not be drought tolerant.
4. *Warmer Months Water Application:* Although a plant is labeled drought tolerant, summer watering is still required. The plant may have low water requirements. Depending upon the plant, drought tolerant plants will look better, thrive and survive the warmer months with infrequent deep watering.
5. *Full Season Plant Water Requirements:* After drought tolerant plants have grown a full season, the water application rate should be diminished, and the drought tolerant plant allowed to survive on less water.
6. *Maintenance:* Drought tolerant and California native plants still need regular maintenance such as pruning, fertilizing, deep watering and pest control.

5.1.2 Irrigation

Landscape irrigation within the Specific Plan area shall be subject to the following and comply with the City's landscape water conservation requirements including but not limited to City of Menifee Municipal Code Chapter 15.04.

- All landscaped areas shall be watered with a permanent underground irrigation system, except for the natural slopes of Granite Hill which shall not be irrigated. Any plant material placed near the Granite Hill nature trail should be able to survive without watering; however, temporary irrigation is permitted if needed for establishing plant material.

- Proper irrigation system design and installation considering seasonal climate extremes of the area is critical to the success of the landscape investment. In particular, the combined summer elements of heat and wind must be carefully considered in proper irrigation design and equipment selection.
- Overhead spray irrigation systems shall be designed with head-to-head 100 percent double coverage at a minimum. All irrigation heads adjacent to walks, drives, and curbs shall be of the pop-up type.
- Irrigation backflow prevention devices and controllers shall be located with minimum public visibility or shall be screened with appropriate plant materials.
- Irrigation systems designed for use with both domestic and reclaimed water are encouraged. All irrigation systems for parks, greenbelts, streetscapes, and common landscaped areas shall be designed to use reclaimed water whenever possible.
- Drip and/or bubbler irrigation will be used where appropriate and moisture sensors and/or central “smart” control irrigation systems shall be incorporated where appropriate.

5.2 Perimeter Streetscape Guidelines

Three public roadways, designated as Enhanced Landscape Corridors by the General Plan, border the Specific Plan boundary; Highway 74 to the north, Meniffee Road to the west, and Briggs Road to the east. Refer to Figure 3-1, *Vehicular Circulation Plan*, for locations. Landscape along public roads that serve as the perimeter of the community help to establish the aesthetic theme of Meniffee Valley as viewed from beyond its boundaries.

- All trees planted in the Highway 74, Meniffee Road, and Briggs Road public rights-of-way shall be a minimum container size of 24-inch box unless the tree species is fast-growing or the tree is not needed for visual screening or to provide shade over a sidewalk or trail.
- All trees and other landscape material in the Highway 74 right-of-way will require an encroachment permit and shall be compliant with Caltrans standards. Caltrans standards shall take precedence over any standards presented in this Specific Plan should a conflict exist.
- Street tree species shall have large canopies that offer cover and shade along sidewalks and trails positioned in and near the public right-of-way.

- Tree spacing shall be approximately 30-feet on center when considering other street trees, the species of tree, driveways, utilities, street light clearances, sight lines, and other constraints. Tree spacing shall consider the potential of larger, varied width parkways which are appropriate for varied, informal massing of trees, although spacing can vary with City approval.
- Street trees and trees in adjacent landscape setback areas shall have deep root systems and shall not bear fruit or have other characteristics that can cause maintenance issues for sidewalks and trails.
- Landscape material in the public right-of-way shall be low maintenance and have low water demand needs.

State Route 74 (Highway 74)

Highway 74 is a 6-lane Expressway and is designated as an Eligible State Scenic Highway and Enhanced Landscape Corridor. Along the MVSP's frontage, which occurs at the north side of Planning Area 13 (Commercial), enhanced landscape provides a scenic aesthetic along this expressway. The streetscape in the public right-of-way will include a parkway varying in width from 12-feet to 47-feet-wide inclusive of a varying width landscaped parkway and a 6-foot-wide sidewalk. Landscape with large canopy street trees will be curb-adjacent with additional trees provided where width allows. Shrubs and groundcover form the understory. All landscape within the public right-of-way shall comply with Caltrans standards. Within the privately-owned property of Planning Area 13, a minimum 10-foot-wide landscape setback will be provided and additional landscape may be provided to adjoin the landscape in the public right-of-way, or a wall or fence with landscape may be provided on the property line depending on the design of development in Planning Area 13. Refer to Figure 5-1, *Highway 74 Streetscape Concept*, for conceptual design.

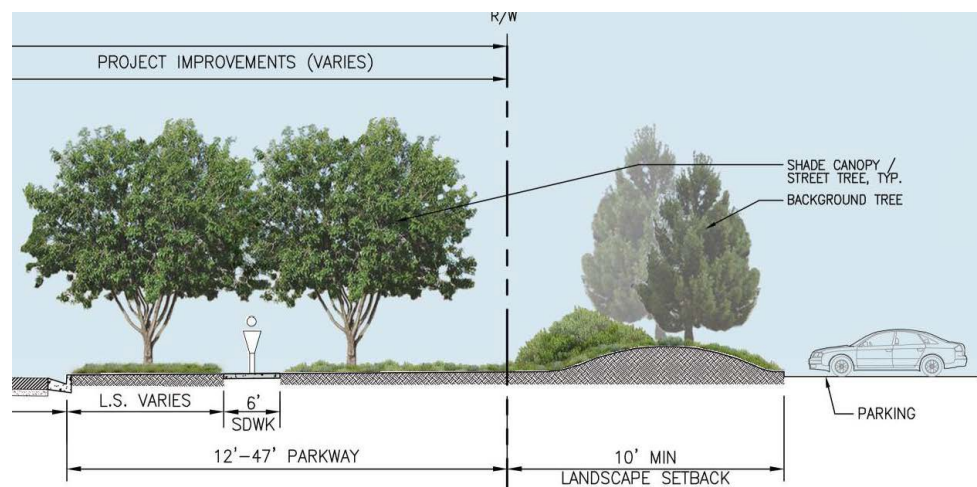


Figure 5-1, Highway 74 Streetscape Concept

Meniffee Road

Meniffee Road borders the western boundary of the MVSP along Planning Areas 10 and 11 (Business Park), and Planning Area 9 (Public Facility - Civic Node). Meniffee Road is classified as a 6-lane Urban Arterial, and also is an Eligible County Scenic Highway and Enhanced Landscape Corridor. The streetscape in the public right-of-way will include a Class II bike lane and a 21-foot-wide parkway inclusive of a 6-foot-wide meandering sidewalk and varying width landscaped parkway. Landscape in the parkway is to include large canopy street trees with shrubs and groundcover, with the trees placed to provide shade and cover for the bike lane. Within the adjacent privately-owned property of Planning Areas 10 and 11, a minimum 5-foot-wide landscape setback will be provided. Land next to the public right-of-way is an ideal location for the placement of functional green spaces, such as detention and water quality basins to make the streetscape appear wider and expansive. Refer to Figure 5-2, *Meniffee Road Streetscape Concept*, for conceptual design.

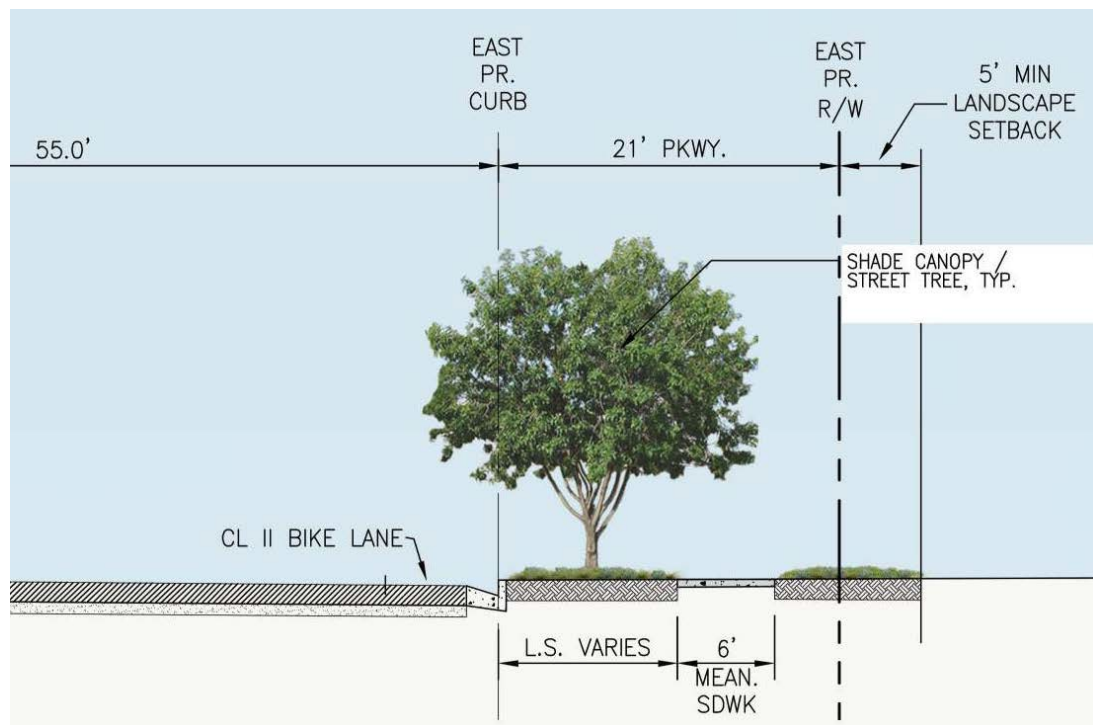


Figure 5-2, Meniffee Road Streetscape Concept

Briggs Road

Briggs Road is classified as a 4-lane Major Road and Enhanced Landscape Corridor that parallels the east side of the MVSP, along Planning Areas 1, 3, and 5 (Residential), Planning Areas 7A (Open Space-Recreation) and 7B (Open Space-Conservation). The streetscape in the public right-of-way will include a Class II bike lane and 21-foot-wide parkway inclusive of a 6-foot-wide meandering sidewalk varying width landscaped parkway. Similar to the other perimeter roads, large canopy shade trees are to be located in the public parkway, with understory of shrubs and groundcover. Beyond the right-of-way in the privately-owned property of Planning Areas 1, 3, 5, and 7A, a 10-foot-wide multi-purpose trail will meander in a 30-foot-wide landscape setback area. Along Planning Areas 1, 3, and 5, the multi-purpose trail will include a decomposed granite (DG) surface and along Planning Area 7A the multi-purpose trail will include a standard pavement surface. Landscape in this area is to include additional trees, shrubs, and groundcover, as well as climbing vines on the perimeter wall that separate private residential lots from the Briggs Road public realm. Refer to Figure 5-3, *Briggs Road Streetscape Concept*, for conceptual design.

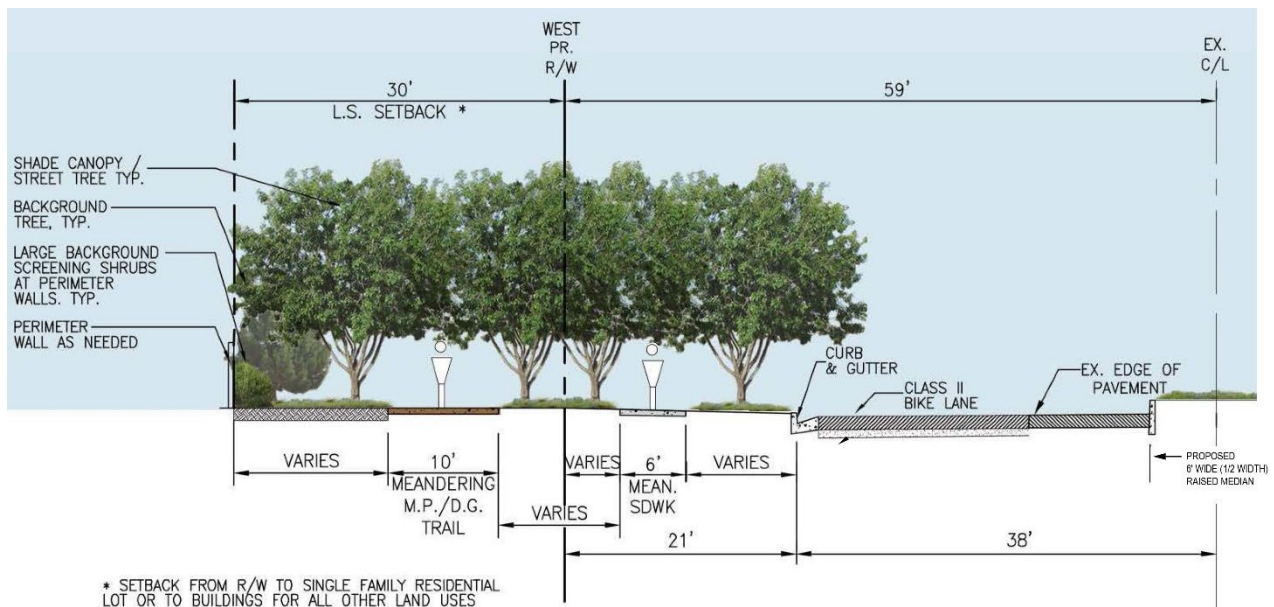


Figure 5-3, Briggs Road Streetscape Concept

5.3 Interior Streetscape and Entry Monument Guidelines

Inside the MVSP area, an internal public street system will serve the Residential community and parks (Planning Areas 1, 2, 3, 4, 5, 6, 7A, and 7B) Business Park, Commercial-Business Park, and Commercial areas (Planning Areas 9, 10, 11, 12, and 13). Refer to Figure 3-1, *Vehicular Circulation Plan*, for the planned conceptual locations of these streets. Streetscape landscape along roads serving residential neighborhoods and parks is intended to be inviting and encourage walking and biking. Streetscape landscape in the employment areas is intended to be complementary but practical for a Business Park environment.

- A primary entry monument for the residential community shall be placed at the intersection of Briggs Road and the Primary Residential Entry Street. Smaller-scale and complementary-designed entry monuments may occur at intersections of Briggs Road and Secondary Residential Entry Streets, however entry monuments in these secondary locations are not required. All trees planted at project entries shall be a minimum 36-inch container box size.
- Primary entry monuments for the business park and commercial areas shall occur in two locations: 1) at the intersection of Highway 74 and Malaga Road that provides access into and through Planning Areas 12 and 13; and 2) at the intersection of Menifee Road and McLaughlin Road that provides access into and through Planning Areas 10 and 11.
- All trees planted along interior streets shall be a minimum container box size of 24-inch box.
- Each residential lot shall receive at least one (1) street tree planted in the right-of-way at the lot's street frontage. This requirement does not apply to flag lots and irregular-shaped lots with small atypical street frontages.
- Corner residential lots shall receive a minimum of two (2) street trees planted in the right-of-way or front yard.
- Tree spacing on all interior public roads shall be approximately 30-feet on center when considering other street trees, the species of tree, driveways, utilities, street light clearances, sight lines, and other constraints. Tree spacing shall consider the potential of larger, varied width parkways which are appropriate for varied, informal massing of trees, although spacing can vary with City approval.
- One (1) primary species of street tree shall be selected for each residential street or neighborhood to strengthen visual identity.
- In Civic, Business Park, Commercial-Business Park, and Commercial areas, trees shall be planted far enough from the roadway pavement to allow clear line-of-sight for trucks and service vehicles.

Primary Residential Entry Street

A Primary Residential Entry Street is expected to provide access into Planning Areas 1, 3, and 6 from Briggs Road. Serving as the main entry into the residential community, this public road is designed to have a grand yet welcoming feel for vehicles, pedestrians, and bicyclists, with one vehicular travel lane in each direction and a raised, 14-foot-wide landscaped center median. Flanking the travel lanes on both sides of the entry road are a Class II bicycle lane within the right-of-way. The southerly side of the right-of-way includes a 15-foot-wide parkway, with a 5-foot-wide meandering sidewalk and 7-feet of curb-adjacent landscaped parkway and a 3 feet of landscape setback beyond the sidewalk. The northerly side of the right-of-way includes a 17-foot-wide parkway, with a 10-foot-wide multi-purpose DG trail and a 7-foot-wide landscape parkway. Beyond the right-of-way, along the north side of the roadway, a minimum 9-foot-wide landscape setback is provided. Canopy shade trees are to be located in the center median and the landscape parkways. Refer to Figure 5-4, *Primary Residential Entry Streetscape Concept*, for conceptual design.

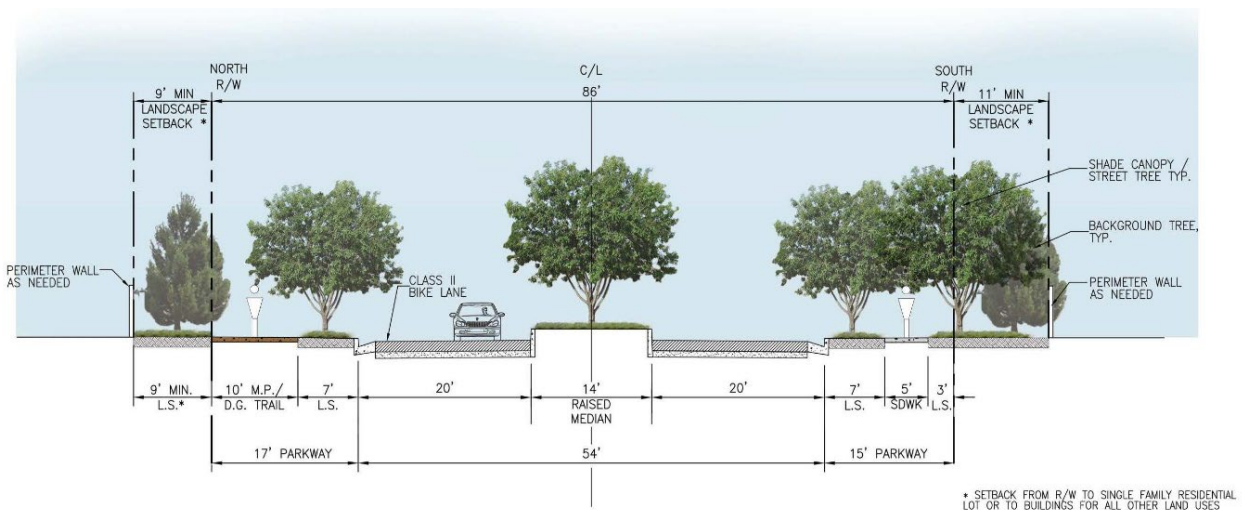
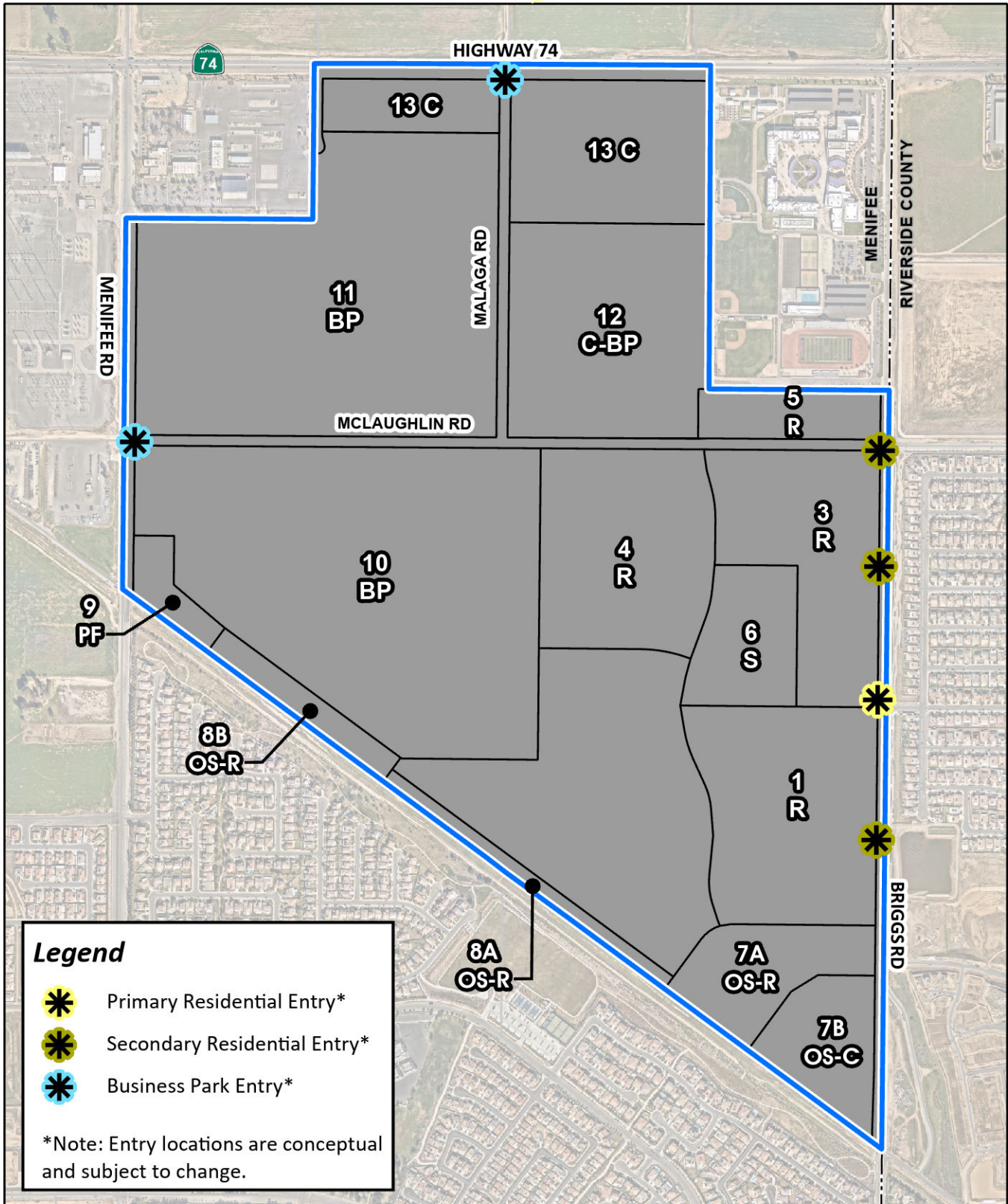


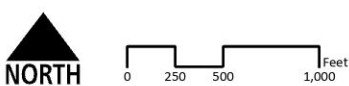
Figure 5-4, Primary Residential Entry Streetscape Concept

A primary residential entry monument is to be located at the intersection of Briggs Road and the Primary Residential Entry Street. Entries into a community visually announce arrival and set the expectation for the character and quality of what is located in the interior. The primary entry monument area, and any secondary entry monument areas placed at intersections of Briggs Road and Secondary Residential Entry Streets shall be designed to create a distinctive visual statement. Landscape includes statement trees such as palm trees that are more formal in appearance than the canopy shade trees that run the length of the public rights-of-way. Accent planting areas include linear rows of shrubs and small plants in the foreground of a low wall that contains signage announcing the name of the community. Three design options are provided herein for the primary entry treatment. Refer to Figures 5-6, 5-7, and 5-8, *Primary Residential Entry, Option 1, Option 2, and Option 3*, respectively.



Source(s): ESRI, RCTLMA (2023), Nearmap Imagery (2023), The Galloway Group (12-10-2021), Hunsaker & Associates (05-03-2022)

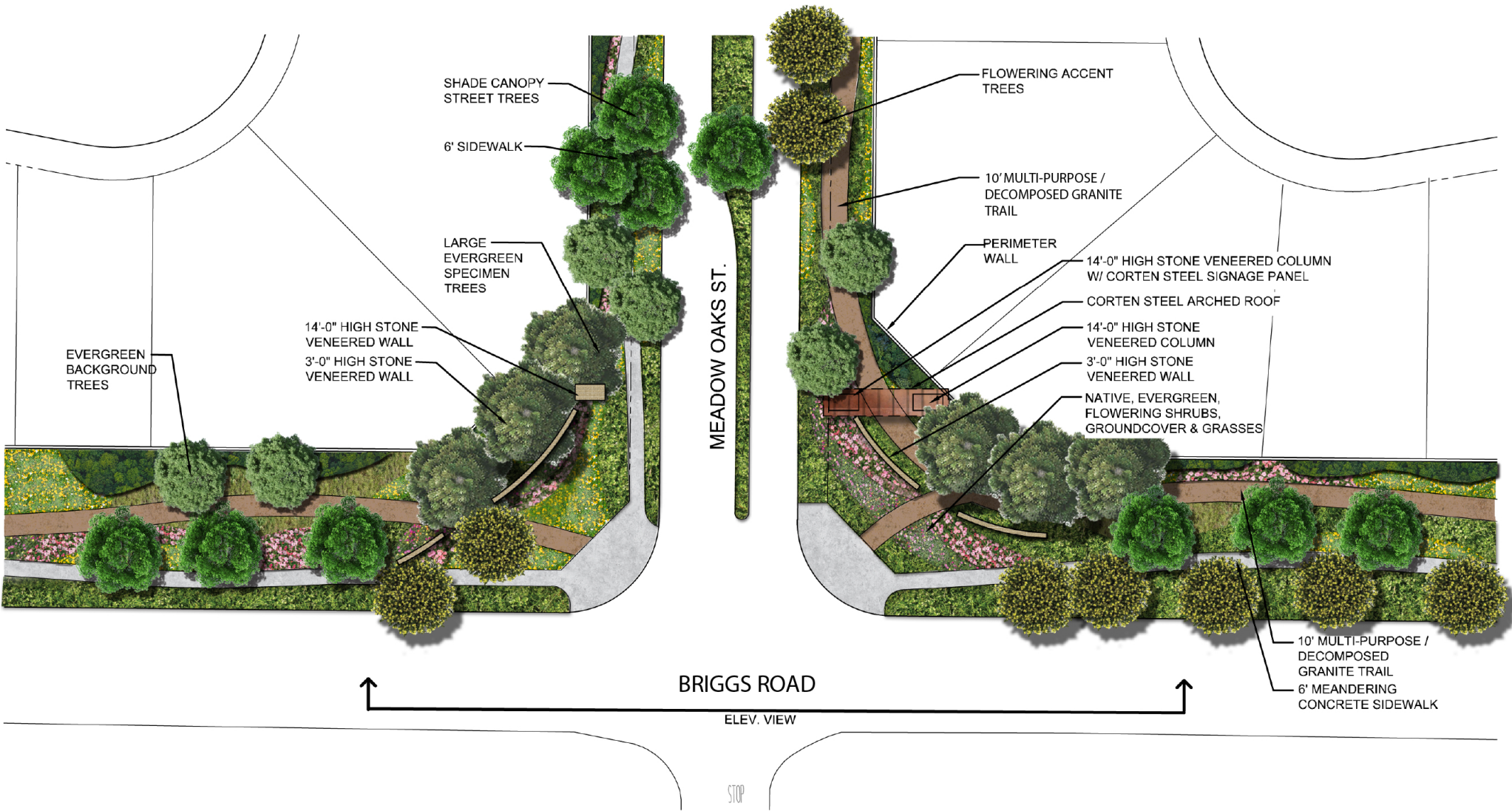
Figure 5-5



Community Entry Locations Key Map

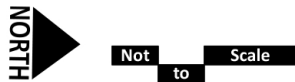


PRIMARY RESIDENTIAL ENTRY ELEVATION



Source(s): Clark and Green (12-28-2021)

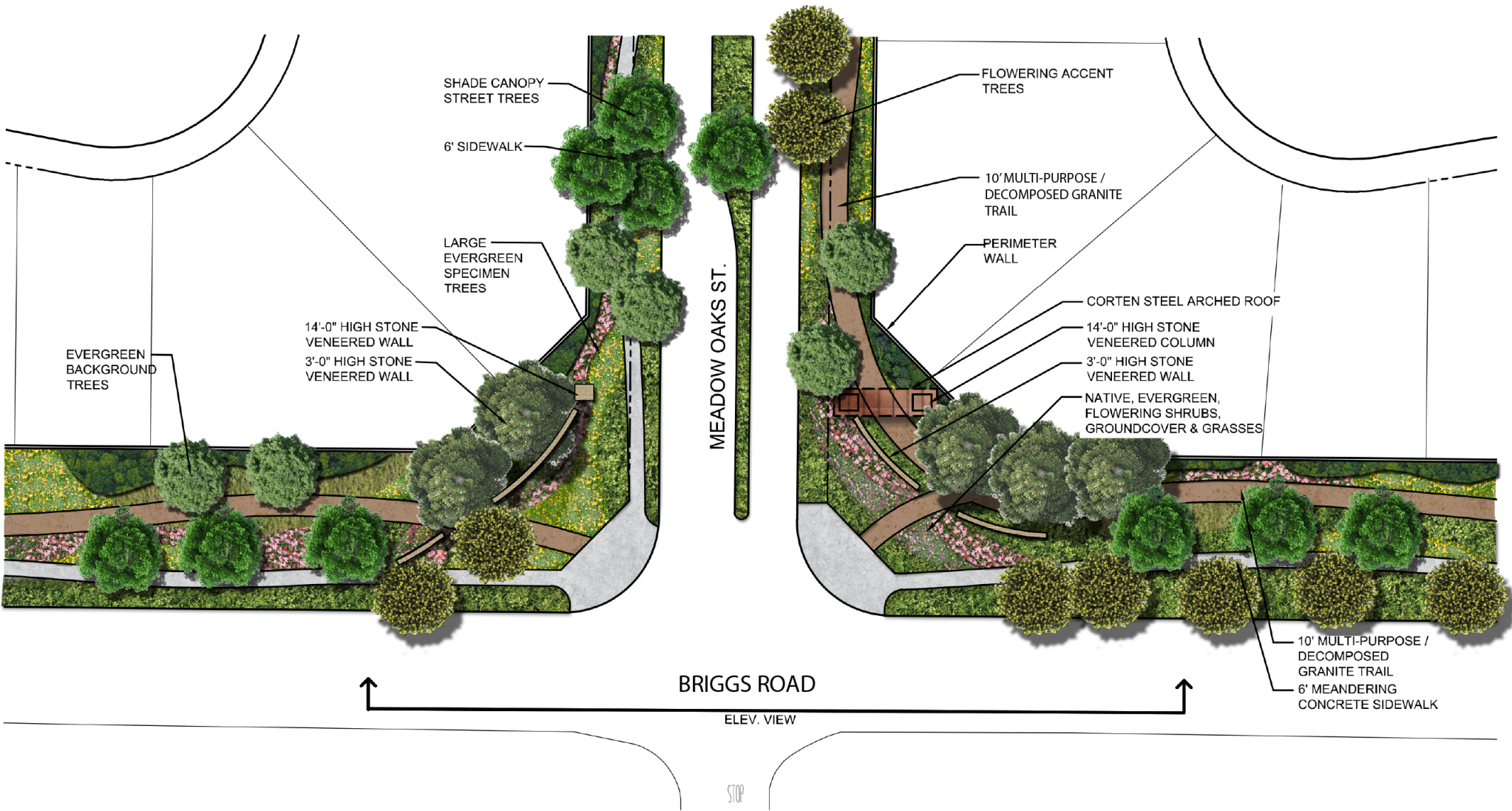
Figure 5-6



Primary Residential Entry Option 1

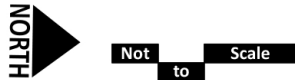


PRIMARY RESIDENTIAL ENTRY ELEVATION



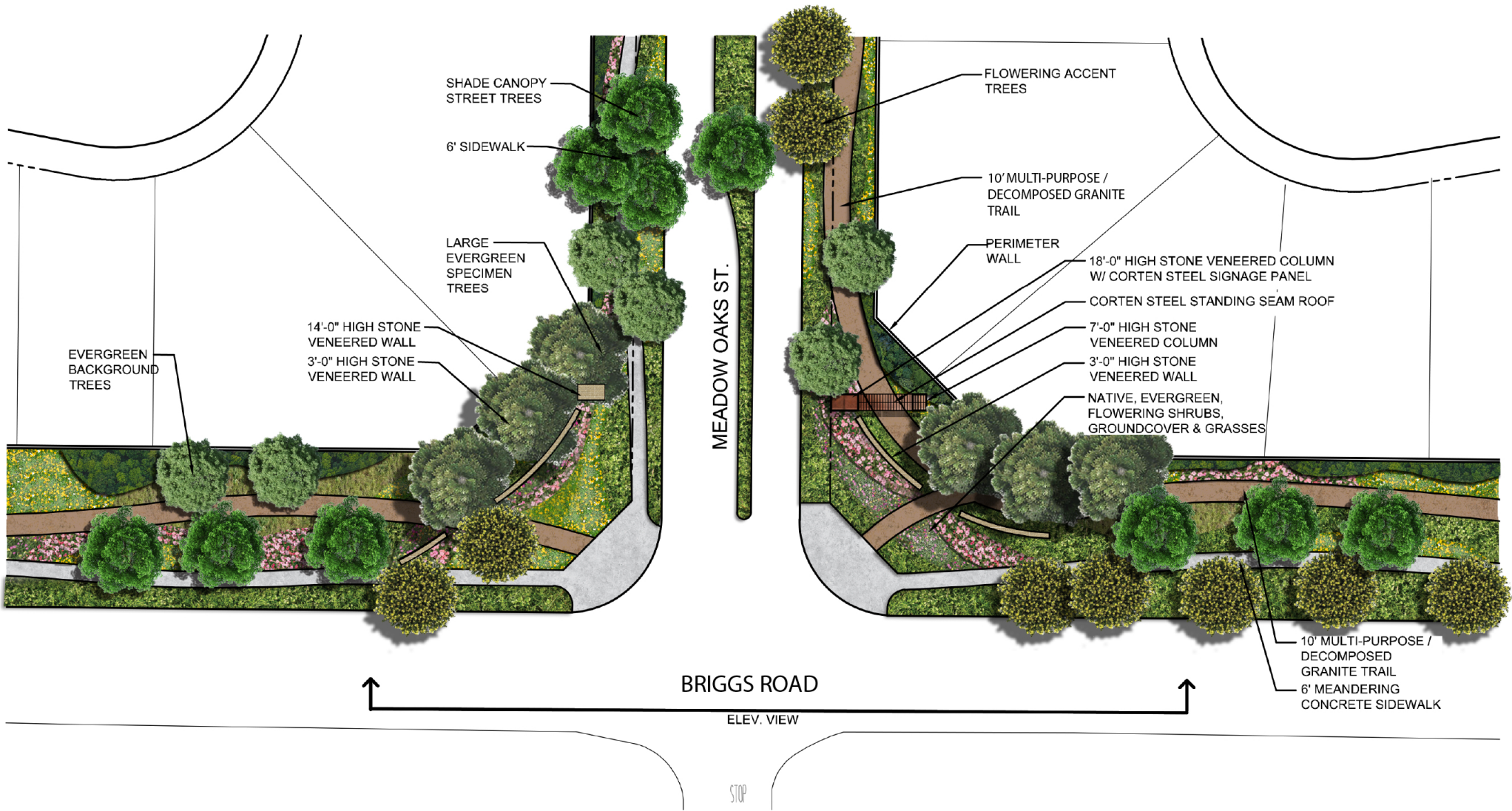
Source(s): Clark and Green (12-28-2021)

Figure 5-7



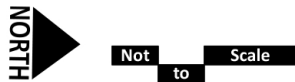


PRIMARY RESIDENTIAL ENTRY ELEVATION



Source(s): Clark and Green (12-28-2021)

Figure 5-8



Primary Residential Entry Option 3

Secondary Residential Entry Streets

Secondary Residential Entry Streets will provide access into Planning Areas 1, 3, 5, and 6 from Briggs Road. Serving as secondary entries into the residential community, these public roads are designed to be narrower and less grand than the primary entry but also inviting to pedestrians and bicyclists to safely enter and exit the community. Secondary Residential Entry Streets have one vehicular travel lane in each direction, no center median, and a Class II bike lane within the right-of-way. The southerly side of the right-of-way includes a 17-foot-wide parkway with 7-feet of curb-adjacent landscape containing canopy shade trees and a 10-foot-wide multi-purpose DG trail. The northerly side of the right-of-way includes a 12-foot-wide parkway with 7-feet of curb-adjacent landscape containing canopy shade trees and a 5-foot-wide multi-purpose DG trail. Main community amenity areas such as the public elementary school site, agri-commercial area, and dog park are expected to be served by a Secondary Entry Street, and focal point landscape is encouraged adjoining the right-of-way where the roads meet these features. Refer to Figure 5-9, *Secondary Residential Entry Streetscape Concept*, for conceptual design.

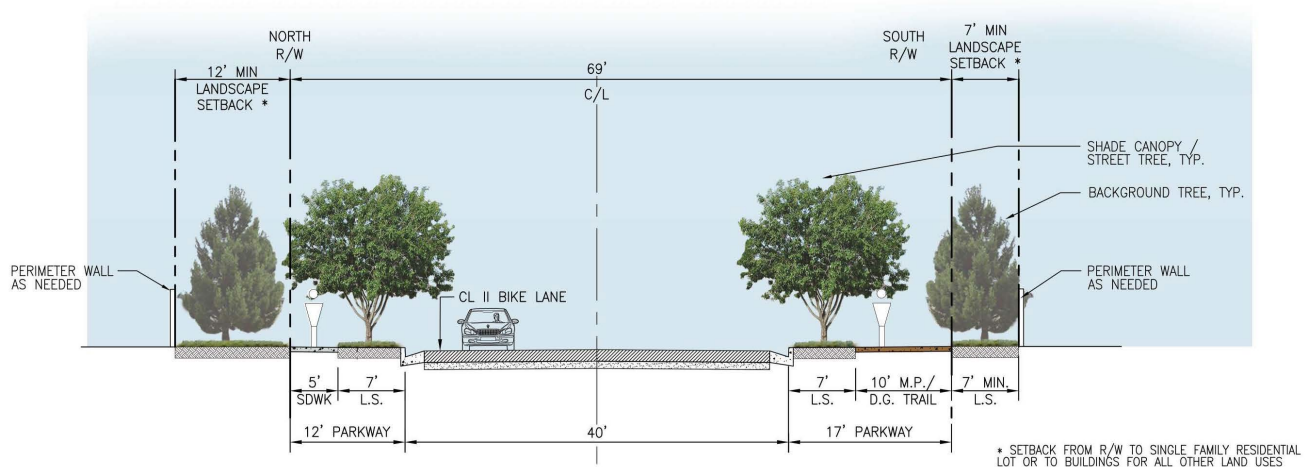


Figure 5-9, Secondary Residential Entry Streetscape Concept

Residential Central Spine Street

A north/south oriented Residential Central Spine Street is envisioned to occur in Planning Areas 1, 2, 3, 4, and 6 to serve as both a vehicular thoroughfare and a major community amenity. The landscape along this street is extensive on both sides and offers pathways and shaded green spaces to be used and experienced as a linear “outdoor room” with filtered light coming through the tree canopies. The Residential Central Spine Street is designed with one vehicular travel lane in each direction, a 14-foot-wide landscaped center median, Class II bike lane within the right-of-way and 15-foot-wide parkways on each side of the street. Behind the minimum 6-foot-wide curb-adjacent landscaped parkway, one side of the public street is designed to include a 5-foot-wide meandering sidewalk and the opposite side is designed to include a 5-foot-wide meandering sidewalk and 10-foot-wide multi-purpose DG trail meandering in a minimum 18-foot-wide private landscape setback zone located beyond the right-of-way. Beyond the right-of-way on the east side of the roadway, a minimum 10-foot-wide landscape setback is provided.

The significant features of the Residential Central Spine Street corridor are the common landscaped setbacks that will occur on both sides of the street beyond the public right-of-way. The private landscaped setback zone on one of side of the street is conceptually planned to be at least 10-feet wide and the other side is planned to be at least 18 feet wide, although these widths may vary at final design. At intersections with all or some of the Primary Residential Entry Street and Secondary Residential Entry Streets, the Residential Central Spine Street may contain roundabouts (traffic circles) with enhanced paving features to slow traffic speeds while providing uninterrupted traffic flow in a pleasing environment. The centers of roundabouts should be landscaped and planted with material that will not obscure lines of sight for drivers in the traffic circle. Refer to Figure 5-10, *Residential Spine Street Streetscape Concept*, for conceptual design.

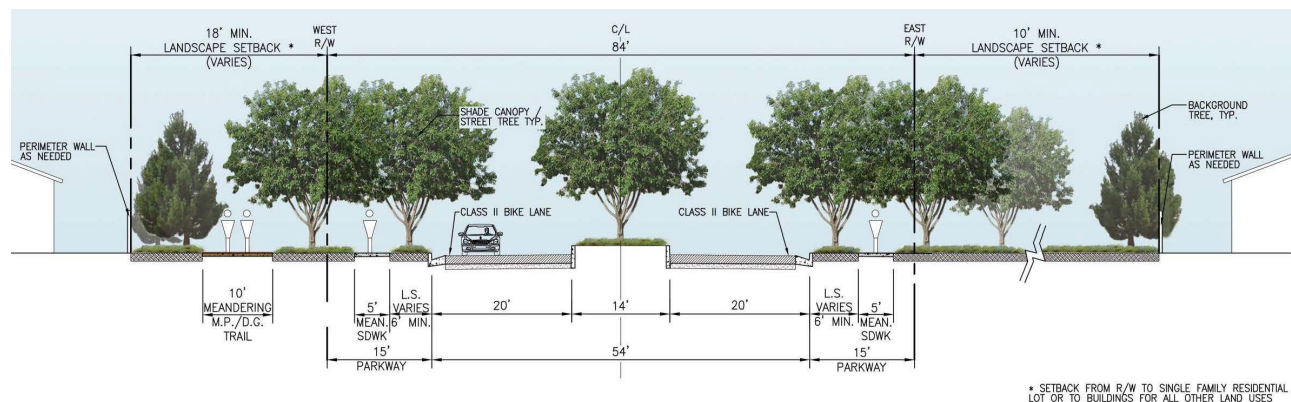
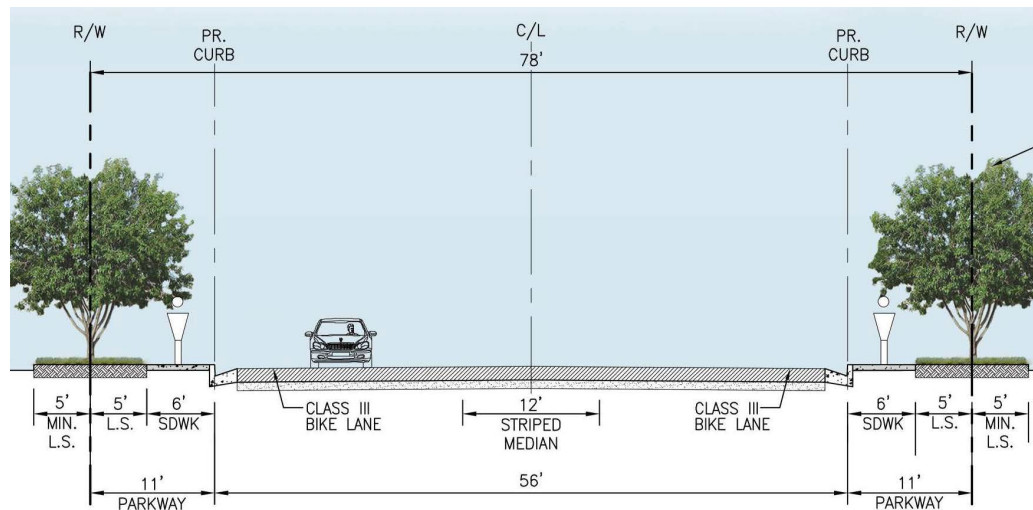


Figure 5-10, Residential Spine Street Streetscape Concept

Malaga Road and McLaughlin Road - Industrial Collector Streets

Malaga Road and McLaughlin Road, designated as Industrial Collector Streets, provide access within the MVSP's Business Park, Commercial-Business Park, and Commercial areas. These streets are designed for commerce and to accommodate the wide turning radii needed for trucks and service vehicles. Because street trees can interfere with lines of sight and cause annoyances for truck drivers, streetscape landscape on Industrial Collector Streets is set back from the curb. Class III bike lanes will be located within the right-of-way. The parkways on each side of Industrial Collector Streets will be 11-feet wide with curb-adjacent 6-foot-wide sidewalks and 5-feet of landscape. Beyond the right-of-way, along both sides of the roadway on Malaga Road and the north side of McLaughlin Road, a minimum 5-foot-wide landscape setback is provided. Street trees will be located on or near the property line and away from the vehicular travel lanes. Canopy shade trees are recommended to provide periodic shade cover over the sidewalks. McLaughlin Road includes a Southern California Edison (SCE) easement partially within the south parkway and partially south of the right-of-way. A 16-foot-wide multi-purpose DG trail meanders in an expansive private landscape zone located beyond the right-of-way. West of Malaga Road, the landscape setback adjacent to the south side of McLaughlin Road will be a minimum of 35-feet-wide. East of Malaga Road, the landscape to the south side of McLaughlin Road will be a minimum of 40-feet-wide. Refer to Figure 5-11 through Figure 5-13, for conceptual design.

**Figure 5-11, Malaga Road Streetscape Concept**

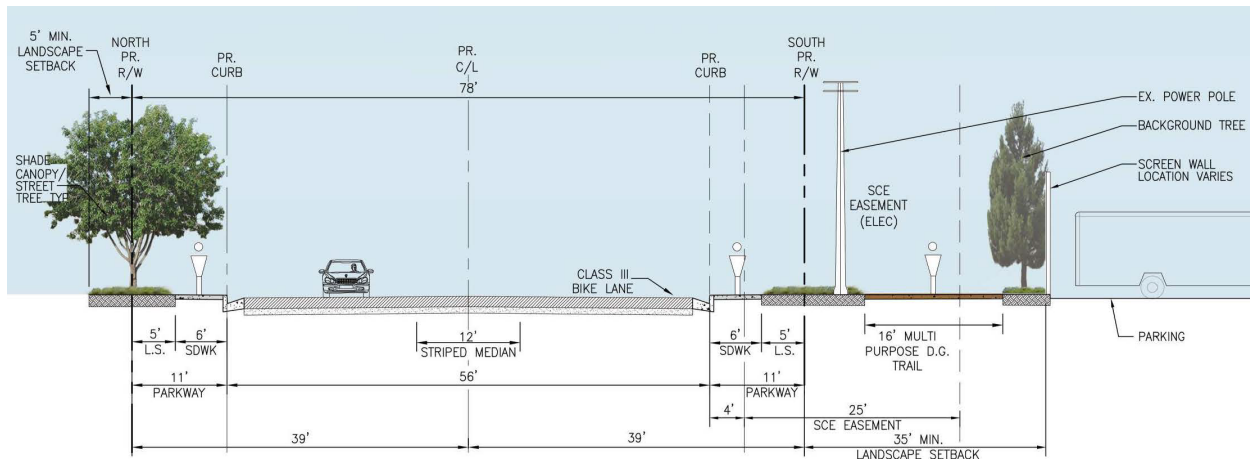


Figure 5-12, McLaughlin Road - West Streetscape Concept

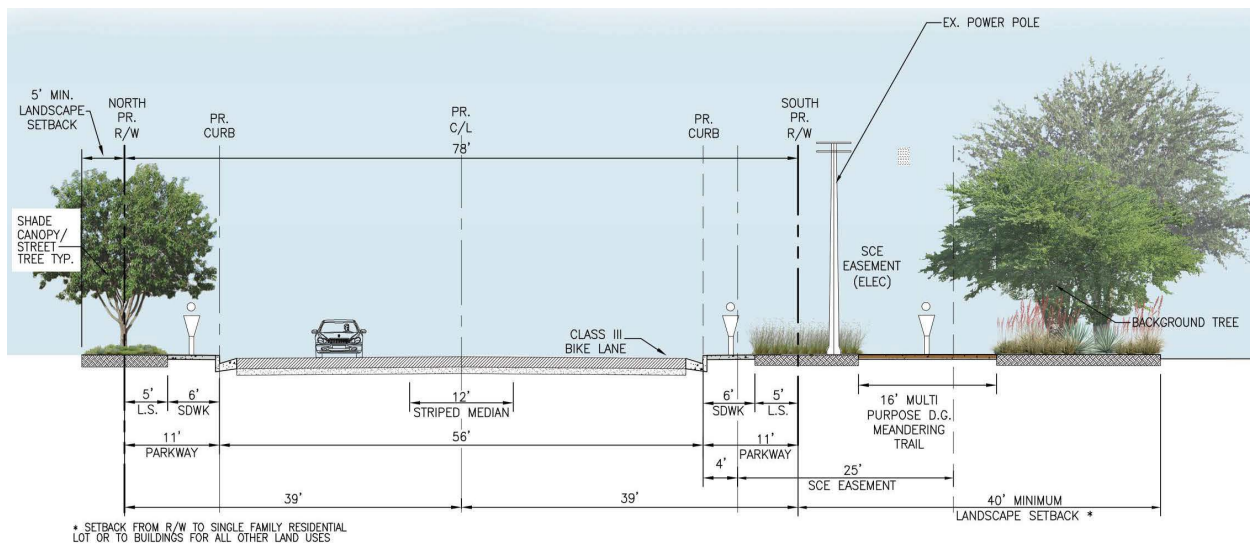
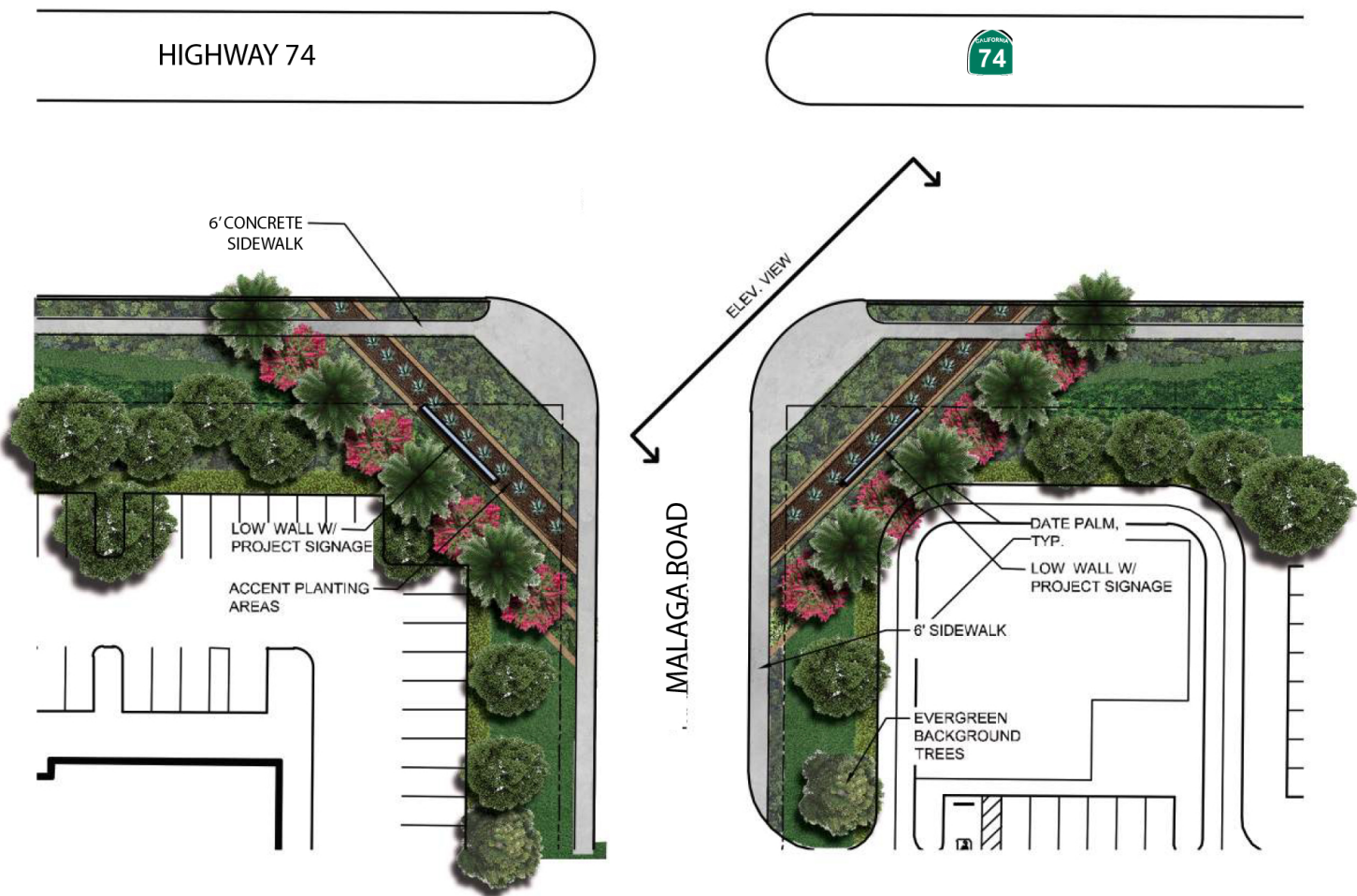


Figure 5-13, McLaughlin Road - East Streetscape Concept

Two primary entry monument locations are planned for the Business Park, Commercial-Business Park, and Commercial areas, with one located at the Malaga Road and Highway 74 intersection, and a second located at the McLaughlin Road and Menifee Road intersection. The landscape concept would emulate but not exactly duplicate the landscape concept for the primary residential entry, with formal accent trees such as palm trees and accent plantings placed in linear rows with a low wall and monument sign announcing the name of the business park. Refer to Figure 5-14, *Business Park Entry Concept*, for conceptual design.



MENIFEE VALLEY BUSINESS PARK ENTRY ELEVATION



*Conceptual Only

Source(s): Clark and Green (05-24-2023)

Figure 5-14

5.4 Edge Treatment and Community Amenity Guidelines

Distinct landscape interfaces, or edge treatments, and landscape in common areas in the residential portion of the community allow functional and practical use of plant material and landscape techniques while also providing visual interest. Each of these edge treatment areas and community amenity areas is discussed in detail below. General guidance is as follows:

- In edge treatment and community amenity areas, landscape for practicality and function as the first priority, and aesthetics as the second priority.
- Where solid walls are desired for separation and screening of land uses, landscape should be included on one or both sides of the wall. If landscape is provided on only one side of the wall, select the side with the more sensitive use. For example, landscape on the residential side of the wall at a residential/business park interface area.
- Trees used for visual buffering shall be evergreen species and spaced close enough in distance to provide a continuous tree canopy at maturity to screen lines-of-sight and minimize artificial light penetration.
- Where distance separation is desired between different land uses, or between land uses and roads, introduce functional landscape features to make good use of the area between these uses. Detention basins, water quality basins, and linear parks or greenbelts are good candidates for the use of this space.
- In common use areas of the residential community (parks, the community recreation center, the elementary school site, etc.), select trees and other plant materials that are colorful and seasonally bloom, with the exception of areas in and around swimming pools. Near pools, trees should be selected that drop minimal leaves, blooms, and other litter to minimize debris from reaching the pool.

5.4.1 Residential Edge Treatments and Amenities

Special landscape treatment considerations are warranted in Menifee Valley's residential area where Residential land uses interface with adjacent Business Park land uses. Also, among other amenities, Planning Area 2 is anticipated to include a private recreation center, Planning Area 6 could include a K-5 school site, and Planning Area 5 could include an agri-commercial area and/or dog park. In the southwest corner of the community are Planning Areas 7A and 7B, the location of a public sports park and passive park on and around Granite Hill. Refer to the guidelines below that address the landscape of these areas and to Figure 5-16, *Residential Edge Treatment and Community Amenity Key Map*, for context.

Briggs Road Interface Treatment

Briggs Road, which will be widened and improved along the MVSP's frontage, forms the eastern boundary of residential Planning Areas 1, 3, and 5. The streetscape in the public right-of-way will include a 21-foot-wide tree-lined landscaped parkway inclusive of a 6-foot-wide meandering sidewalk. Large canopy shade trees are to be located in the public parkway, with understory of shrubs and groundcover. Beyond the right-of-way in the privately-owned property of Planning Areas 1, 3, and 5, a 10-foot-wide multi-purpose DG trail will meander in a 30-foot-wide landscape setback area. Along Planning Area 7A the multi-purpose trail will meander and will include a standard pavement surface. Landscape in this area is to include additional trees, shrubs, and groundcover, as well as climbing vines on the perimeter wall that separate private residential lots from the Briggs Road public realm. Refer to Figure 5-15, *Conceptual Residential Edge Design at Briggs Road*, for the conceptual interface.

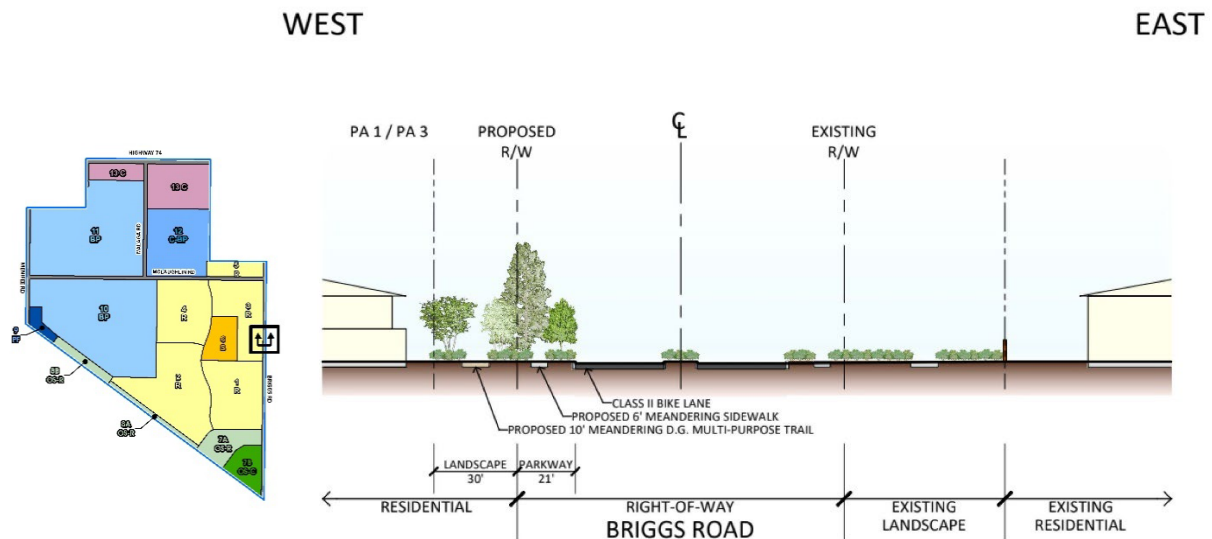


Figure 5-15, Conceptual Residential Edge Design at Briggs Road

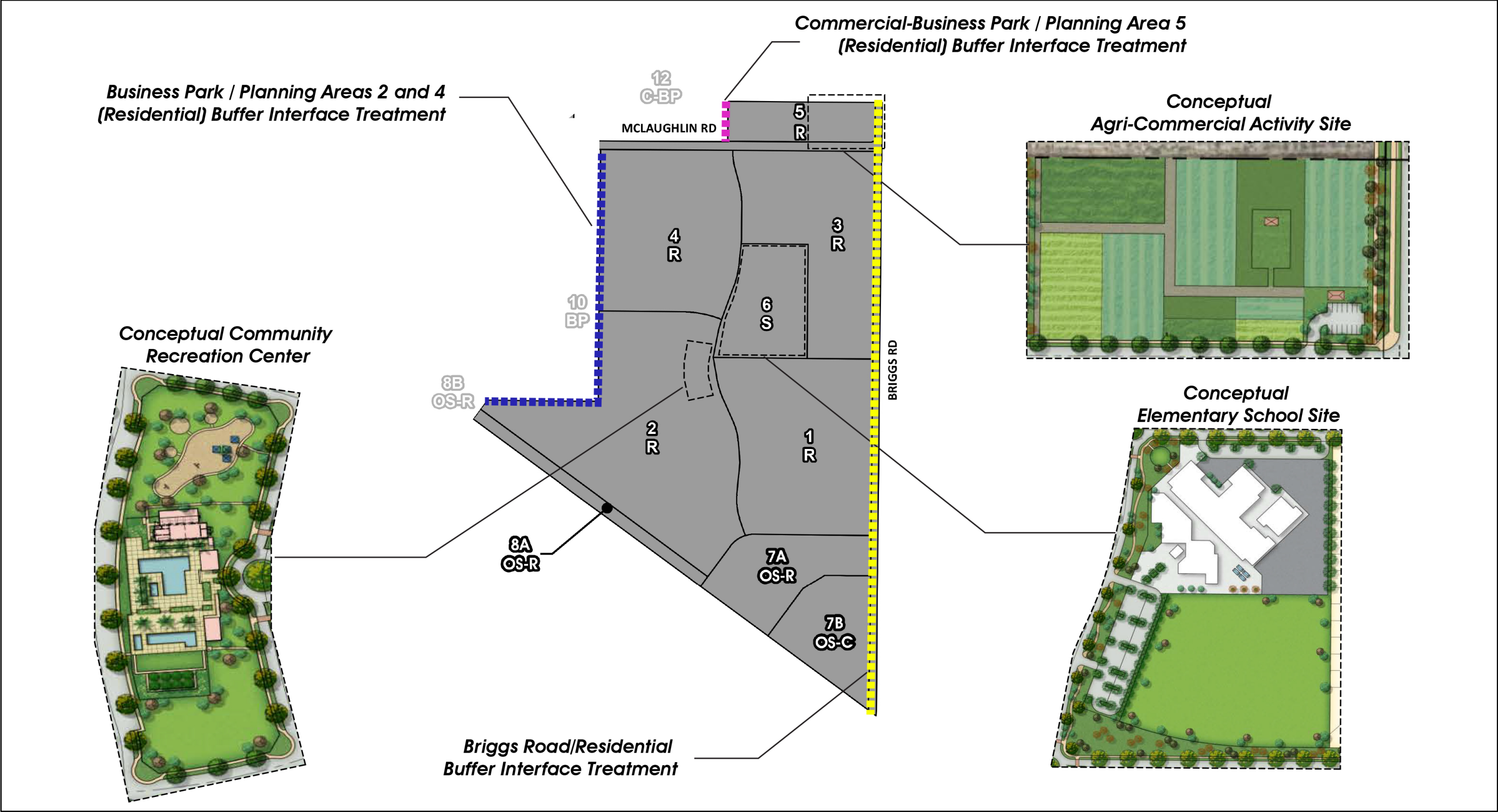
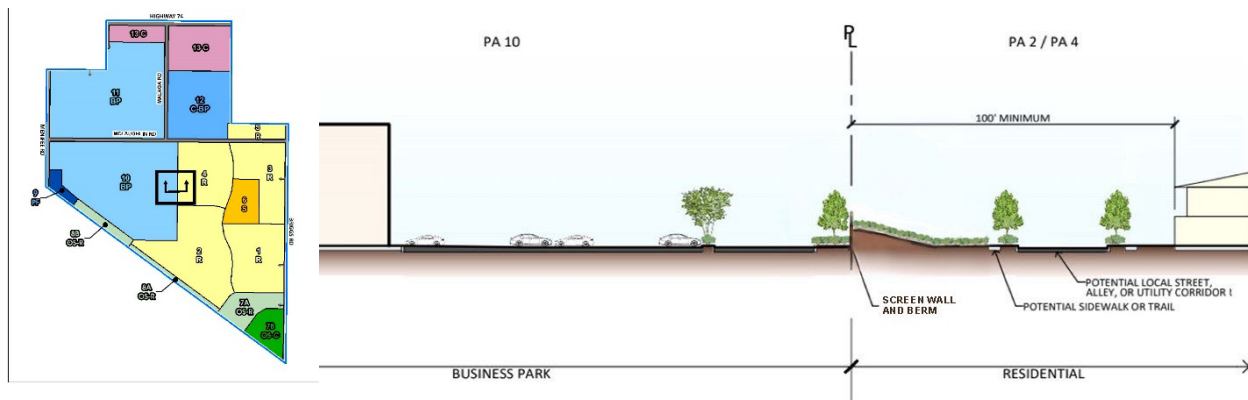


Figure 5-16

Business Park/Residential Buffer Interface Treatment

Along the west side of Planning Areas 2 and 4, where the Residential land use designation meets the Business Park land use designation in Planning Area 10, a buffering treatment is needed to physically separate the two uses. At the Business Park property line, a solid screen wall should occur, with the minimum height determined by a noise study to ensure that operational noise levels in the business park are attenuated to meet City of Menifee noise requirements for nearby residential uses. Residential buildings are envisioned to be setback approximately 100 feet from the Business Park property line. The area between the residential units and the western boundary will include landscaping and could include streets, alleys, utility corridors, sidewalks, multipurpose trails, and other amenities in the spirit of a linear green belt park to make beneficial use of the space for use by community residents. Trees planted in this area are to be evergreen and closely spaced to provide a continuous canopy at maturity to visually screen uses and lighting in the Business Park. Refer to Figure 5-17, *Conceptual Residential Interface at Business Park Edge*, for the conceptual interface.



*Note: The berm was added per the Planning Commission hearing on 1/10/24.

Figure 5-17, Conceptual Residential Interface at Business Park Edge

Public Sports Park and Granite Hill Concept Design

Open Space-Recreation (OS-R) and Open Space-Conservation (OS-C) land uses are planned in the southeastern portion of the MVSP area in Planning Areas 7A and 7B, respectively. This area is designated for a public sports park and passive open space park area that includes the preservation of Granite Hill. A design concept for Planning Areas 7A and 7B is shown in Figure 5-18, *Public Sports Park and Granite Hill Concept Design*. As shown, features are conceptually designed to include soft-surface sports fields, hard surface sport courts, sensory garden, picnic shelters, playground, aquatic center including a swimming pool and splash pad, a parking area, and restroom. A design concept for the aquatic center portion of park is provided in Figure 5-19, *Public Aquatic Center Design Concept*. The final selection of this public park and its recreation features will be determined by the City of Menifee. Landscape in the sports park

will be multi-functional with plant species and planting locations selected for functionality and aesthetic enhancement of the park.

The passive open space park on and around Granite Hill is designed to be kept in its natural condition, with a nature trail located on the hill in the same location where an informal trail existed at the time this Specific Plan was prepared. No irrigation is permitted on the slopes of Granite Hill, unless temporary irrigation is needed to establish initial plant growth. Also, no landscape should occur on the slopes of Granite Hill other than non-invasive drought tolerant plant material that may be needed in select locations next to the trail to deter trail users from wondering off the trail and into the natural area.

Community Recreation Center Concept Design

A private community recreation center managed by a homeowners' association (HOA) is expected in Planning Area 2. A design concept is shown in Figure 5-20, *Private Community Recreation Center Concept Design*. Features are conceptually designed to include a tot lot, open lawn area, and swimming pool area with shade structures and a restroom building. The location of the community recreation center and final election and design of the amenities will be determined by the Master Developer of Planning Area 2 in consultation with City at the time residential subdivision maps are brought forward. Landscape in the community recreation center area is to be colorful, with blooming trees, shrubs, and vines, and with the primary tree species being different than the tree species used as street trees on adjoining residential streets.

Elementary School Site Considerations

Approximately 12.0 net acres in Planning Area 6 are allowed to develop as an elementary school site by the Romoland School District. The location of the school site will be determined by the Master Developer of the MVSP residential community in consultation with the Romoland School District and City of Menifee and in accordance with CCR Title 5, Division 1, Chapter 13, Subchapter 1, Article 2. In addition to the school buildings, facilities, and parking, the elementary school site is expected to include outdoor play areas and sports fields. Landscape on the school site also will be determined by the School District, but the plant species must be selected from the plant palette provided in Table 5-A of this Specific Plan.

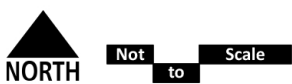
Agri-Commercial Area Considerations

Agri-Commercial activity is a permitted use in Planning Areas 5, 12, and 13. This use would consist of urban agricultural activities focused on crops and animal husbandry. The plant material is expected to be agricultural, although ornamental landscape is expected around the perimeter of the area and in focal point areas such as the entry, and in any on-site parking areas.



Source(s): Clark & Green (August 2023)

Figure 5-18





Source(s): Clark & Green (July 2023)

Figure 5-19

Not
to
Scale



*Conceptual Only

Source(s): Clark & Green (August 2023)

Figure 5-20

Not to Scale

5.4.2 Business Park, Commercial, and Commercial-Business Park Edge Treatments

Special landscape treatment considerations are warranted at perimeter edges of Planning Areas 10, 11, 12, and 13 where the Business Park, Commercial, and Commercial-Business Park developments meet perimeter roads, residential areas, off-site Heritage High School, Malaga Road, McLaughlin Road, and greenbelts located in Planning Area 8B. Refer to the guidelines below that address the landscape of these areas and to Figure 5-22, *Business Park Edge Treatment Key Map*. Note that landscape planters are not to be located in truck courts and loading dock areas to facilitate maneuverability of trucks and tractor trailers in these areas. Landscape requirements in business parks shall be met outside of the truck courts.

Menifee Road Interface Treatment

Menifee Road, which will be widened and improved along the MVSP's frontage as part of adjacent development, forms the western boundary of Business Park Planning Areas 10 and 11. The streetscape in the public right-of-way will include a Class II bike lane and a landscaped 21-foot-wide parkway which includes a 6-foot-wide meandering sidewalk. Although additional expansive landscape in the privately-owned areas of Planning Areas 10 and 11 is not required, the western edges of these planning areas are preferred locations for functional green spaces, such as detention and water quality basins, to make the streetscape along Menifee Road appear wider and expansive. As shown below, if basins are located in this area, trees and shrubs should occur at varying widths around the perimeter to enhance the aesthetic of the Business Park as viewed from Menifee Road. Refer to Figure 5-21, *Conceptual Business Park Edge Design at Menifee Road*, for the conceptual interface.

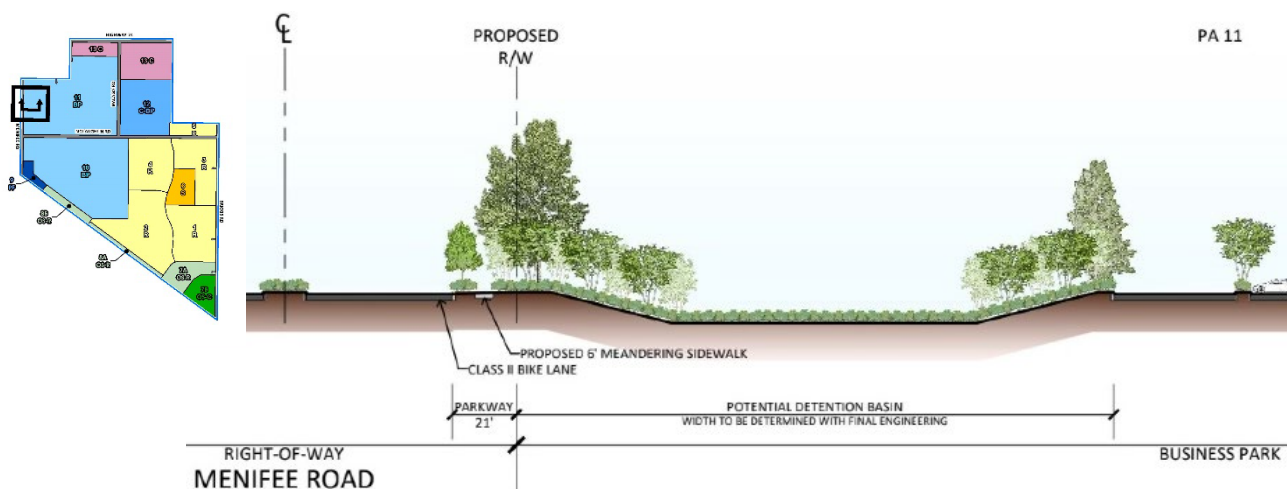
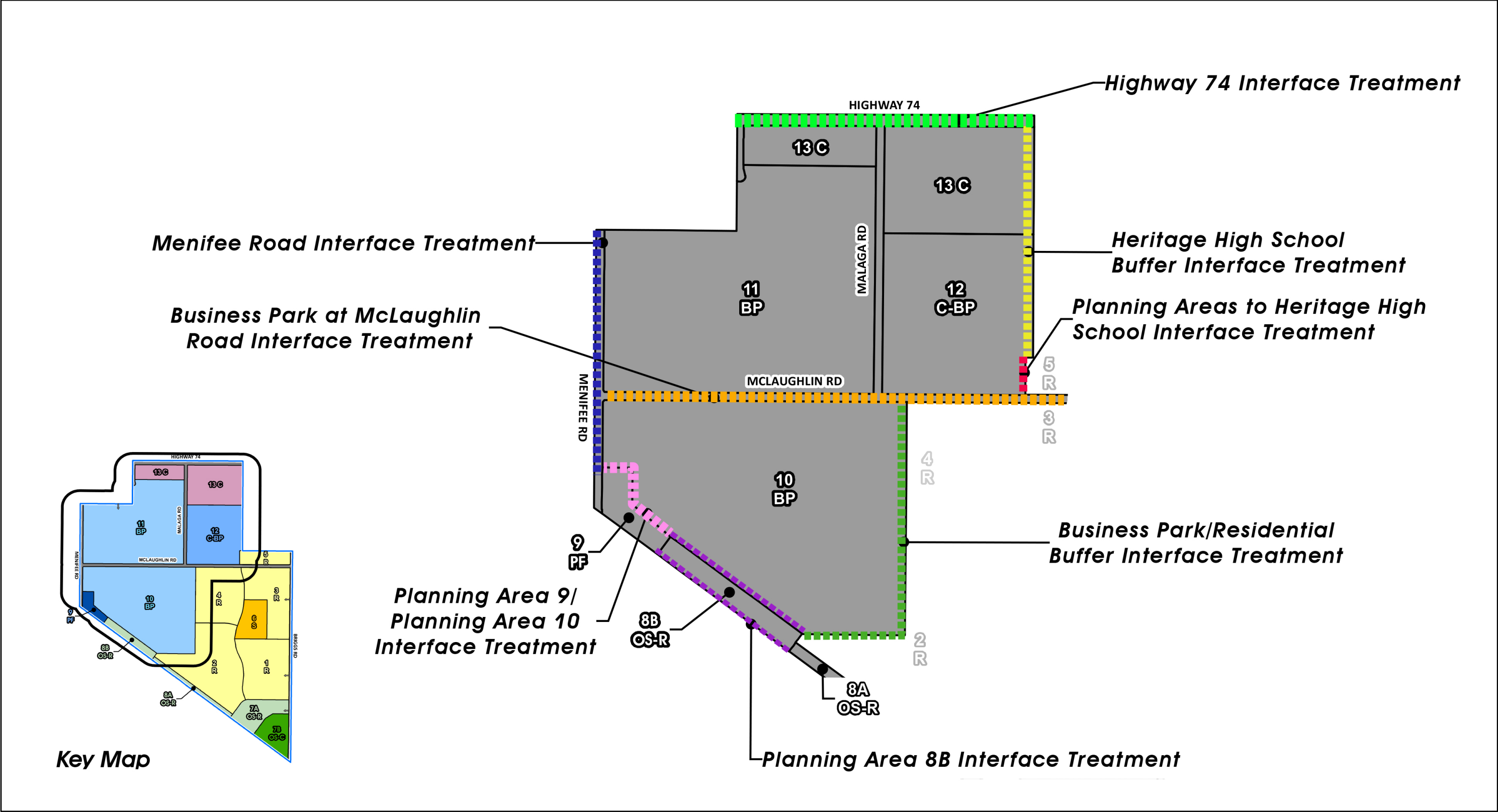


Figure 5-21, Conceptual Business Park Edge Design at Menifee Road



Source(s): ESRI, RCTLMA (2021), The Galloway Group (12-10-2021)

Figure 5-22

Highway 74 Interface Treatment

Highway 74, which will be improved along the MVSP's frontage as part of adjacent development, forms the northern boundary of Planning Area 13. The streetscape in the public right-of-way will include a tree-lined landscaped parkway varying in width from 12-feet to 47-feet-wide inclusive of a 6-foot-wide meandering sidewalk. There is a minimum 10-foot-wide landscape setback required at the northern boundary of Planning Area 13, and parking lot tree planters that adjoin the public right-of-way are encouraged to be staggered with the placement of Highway 74 street trees to convey a thoughtful placement of trees and landscape. All landscape within the public right-of-way shall comply with Caltrans standards. Refer to Figure 5-23, *Conceptual Commercial Edge Design at Hwy 74*, for the conceptual interface.

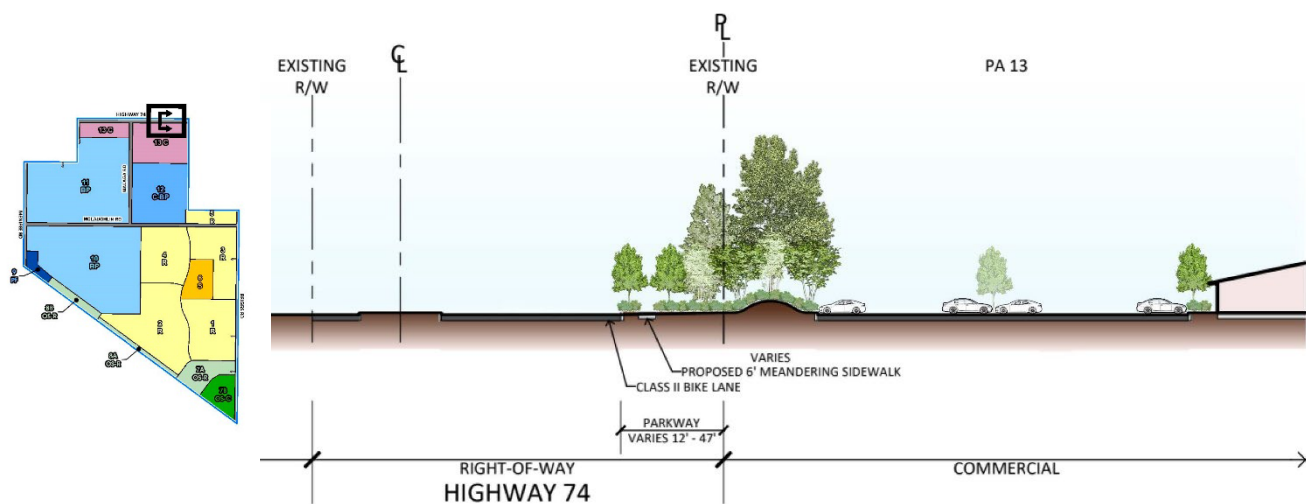


Figure 5-23, Conceptual Commercial Edge Design at Hwy 74

Heritage High School Buffer Interface Treatment

To the east of Planning Areas 12 and 13 is the campus of Heritage High School, a public high school of the Perris Union High School District. Nearest to the property boundary is the school's sports fields. Both a physical separation barrier and buffer distance should occur between the high school property and business park uses in Planning Area 12 and potentially commercial uses in Planning Areas 12 and 13. The eastern edges of Planning Areas 12 and 13 are envisioned to include a minimum 87-foot-wide landscape buffer. Trees planted in this area are to be evergreen and closely spaced to provide a continuous canopy at maturity to visually screen business park uses from the high school property.

In the buffer area along the eastern edges of Planning Areas 12 and 13, a solid screen wall should be placed between business park and commercial uses and the high school property, with the minimum height determined by a noise study to ensure that operational noise levels in the business park are attenuated to meet City of Menifee noise requirements for schools. However, for permitted uses in Planning Areas 12 and 13 that have land use compatibility with

the high school, such as commercial uses, business incubator uses, agri-commercial uses, residential uses, or other compatible use as determined by the City of Menifee, a solid wall between all or a portion of Planning Areas 12 and 13 and the landscaped buffer area would not be required. Refer to Figure 5-24, *Conceptual Commercial-Business Park Edge Design at High School*, for the conceptual interface.

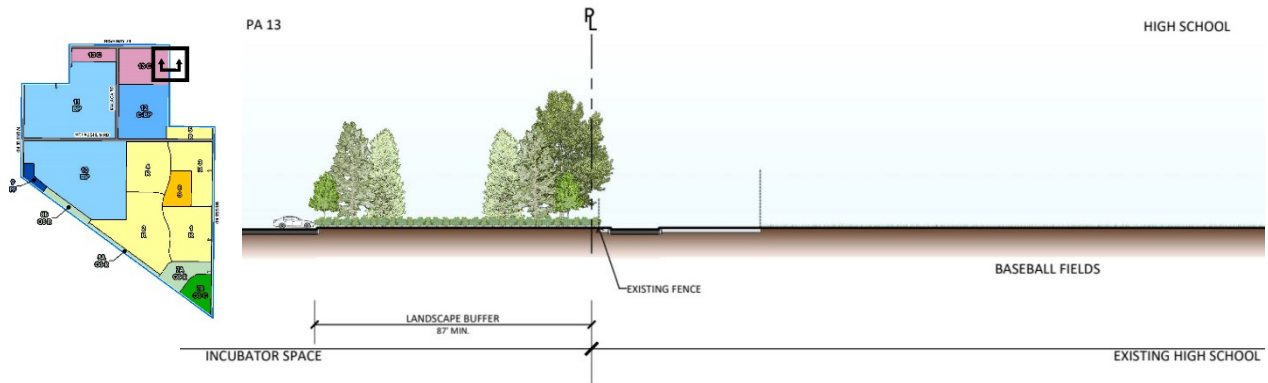


Figure 5-24, Conceptual Commercial-Business Park Edge Design at High School

Business Park/Residential Buffer Interface Treatment

Residential development will occur east and southeast of Business Park Planning Area 10 and near the southeast corner of Planning Area 12. A buffer interface treatment will occur on the residential side of the property line as described above in Subsection 5.4.1. As such, no special treatment is required on the Business Park side of the property line other than a screen wall to be located on the property line. To reduce the perceived height of the wall on the residential side of the property line, the grade of the business park is encouraged to be lower, so the height of the wall is taller on the business park side of the wall than the residential side of the wall. Refer to Figure 5-25, *Conceptual Business Park Screen Wall Edge Design at Residential*, for the conceptual interface.

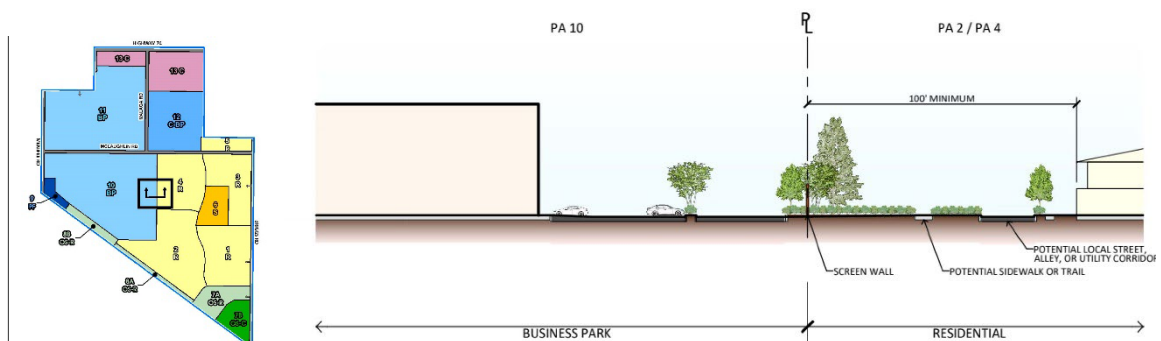


Figure 5-25, Conceptual Business Park Screen Wall Edge Design at Residential

Business Park at McLaughlin Road Interface Treatment

McLaughlin Road, runs east/west through the center of the MVSP and separates Planning Areas 10 and 11, both designated Business Park. Special consideration is to be given to the interface across McLaughlin Road. As shown below, screen walls should be provided at the property lines of business park uses where they abut McLaughlin Road to separate the business park uses from existing overhead power lines and other uses occurring in the McLaughlin Road right-of-way. Landscape treatment should occur outside the McLaughlin Road right-of-way. A detention basin could occur within Planning Area 11 adjacent to McLaughlin Road. Refer to Figure 5-26, *Conceptual Business Park Edge Design at McLaughlin Road*, for the conceptual interface.

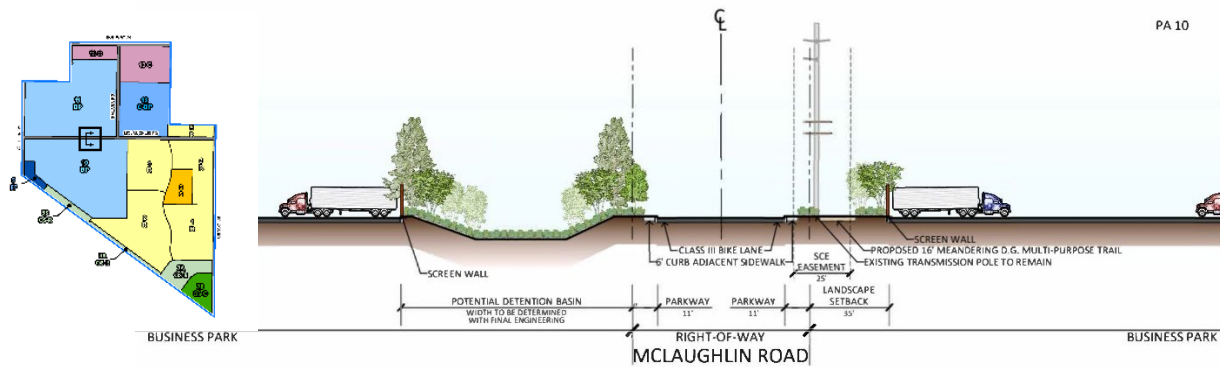


Figure 5-26, Conceptual Business Park Edge Design at McLaughlin Road

Planning Area 8B Interface Treatment

At the southern edge of the Specific Plan area is a utility easement, which will be utilized as a greenbelt with multi-purpose DG trail (Planning Areas 8A and 8B). The interface between the business park uses and the greenbelt should be open with no physical barrier except as might be needed to secure areas of the business park that require enclosure for safety and security of the business operation or to screen loading docks or truck parking. To reduce the perceived wall height in adjacent residential areas, the finished building pad areas in Planning Area 10 can be designed to occur at a lower elevation than adjacent residential areas, thereby making the visible screening lower as viewed from residential areas. Alternatively, a landscaped berm can be used to screen loading docks or truck parking. Landscape in Planning Area 8B should include functional uses such as detention basins and water quality basins, in addition to trees, shrubs, and groundcover. Refer to Figure 5-27, *Conceptual Business Park Edge Design at Planning Area 8B*, for the conceptual interface.

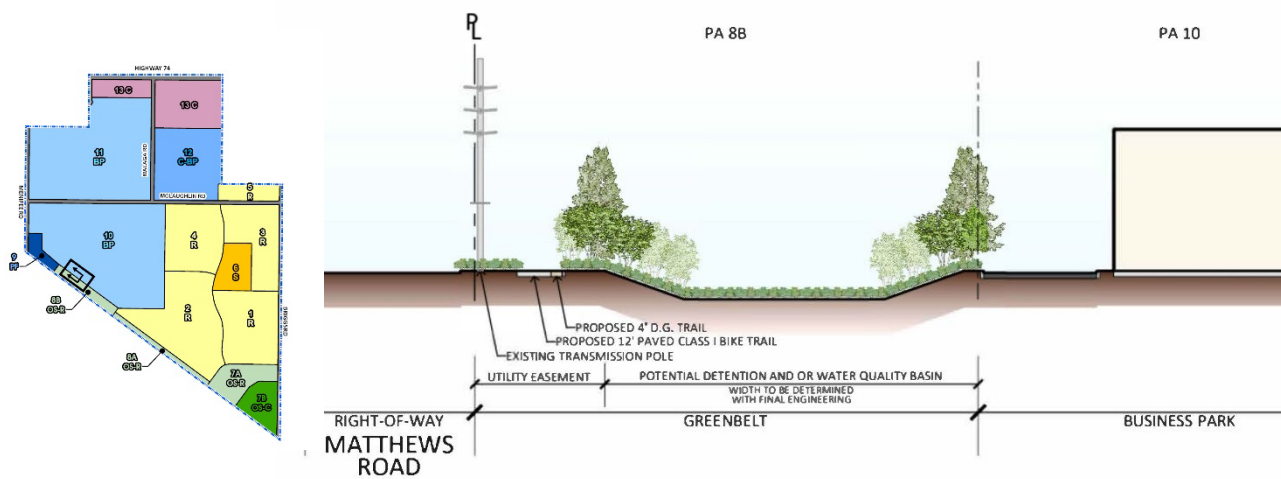


Figure 5-27, Conceptual Business Park Edge Design at Planning Area 8B

Planning Area 5 to Heritage High School Interface Treatment

To the north of Planning Area 5 is the campus of Heritage High School, a public high school of the Perris Union High School District. Nearest the southern property boundary of the school is the school's drainage channel, which is adjacent to the existing high school parking lot. As such, no special interface treatment is needed on the north side of Planning Area 5 other than a physical barrier such as a wall (for residential adjacency) or a wire mesh fence (for agri-commercial, dog park, or other amenity adjacency). Refer to Figure 5-28, *Conceptual Planning Area 5 Edge Design at High School*, for the conceptual interface.

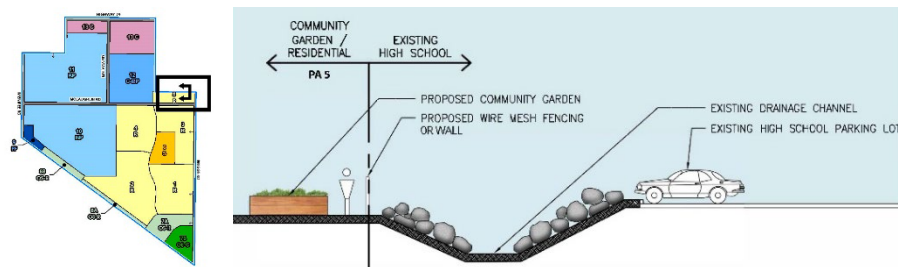


Figure 5-28, Conceptual Planning Area 5 Edge Design at High School

Planning Area 9/Planning Area 10 Interface Treatment

Planning Area 9 is anticipated to be developed with a public facility use such as a fire station or other similar use, and will abut Business Park uses in Planning Area 10. Because these uses are generally compatible, the interface treatment can be limited to a landscape strip on either or both sides of the planning area boundary and a privacy wall on the property line to separate the business park uses from the proposed fire station. Refer to Figure 5-29, *Conceptual Business Park Edge Design at Planning Area 9*, for the conceptual interface.

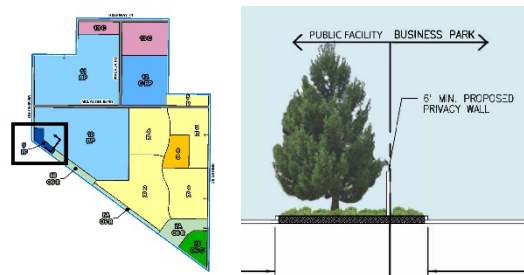


Figure 5-29, Conceptual Business Park Edge Design at Planning Area 9

5.5 Walls and Fence Guidelines

Walls and fencing used in the Specific Plan area are intended to complement the overall site and landscape design, strengthen community identity, and provide visual and physical privacy site lines for views and buffering between different land uses. This section describes the various walls and fences, and their main intent. Illustrations are provided for general reference.

- Walls and fences should be kept as low as possible while performing their necessary sound attenuation, screening and security functions.
- Use landscape such as clinging vines and shrubs to soften the appearance of fencing and walls. Walls not covered with landscape should have anti-graffiti treatments.
- Perimeter walls located along public streets and open space areas abutting Residential and Business Park planning areas should provide for decorative columns or pilasters every 100 feet.
- Along Briggs Road adjacent to Planning Area 7B, a retaining wall with a maximum height of 50 feet shall be permitted.
- Solid walls should be used to screen truck courts, loading docks, and utilitarian uses in Business Park, Commercial-Business Park, and Commercial Planning Areas from adjacent uses. Solid walls at these locations should include pilasters or physical breaks or undulations in the horizontal plane of the wall at regular intervals.
- Where freestanding walls are used at property frontages, or screens are used to conceal storage and equipment areas, they should be designed to blend with the site's architecture and use landscape in combination with such walls.
- Permitted wall materials include decorative concrete block, brick, natural stone/stone veneer, precast concrete panels, stucco finished walls or other unique wall materials or finishes that integrate well with on-site buildings.
- Walls may be left natural or covered with stucco, except precision block must be covered with stucco.
- All walls must feature matching or complimentary cap materials.
- Prohibited wall and fencing materials include the following: barbed wire, electrically charged fencing, plain exposed precision block, corrugated metal, chain link, and fencing using plastic materials. However, vinyl-coated chain link and wire mesh fencing may be appropriate in non-visible interior locations of business park and commercial-

business park areas and at the northern boundary of Planning Area 5 adjacent to the existing drainage channel on the Heritage High School property if approved by the City of Menifee as part of implementing development projects.

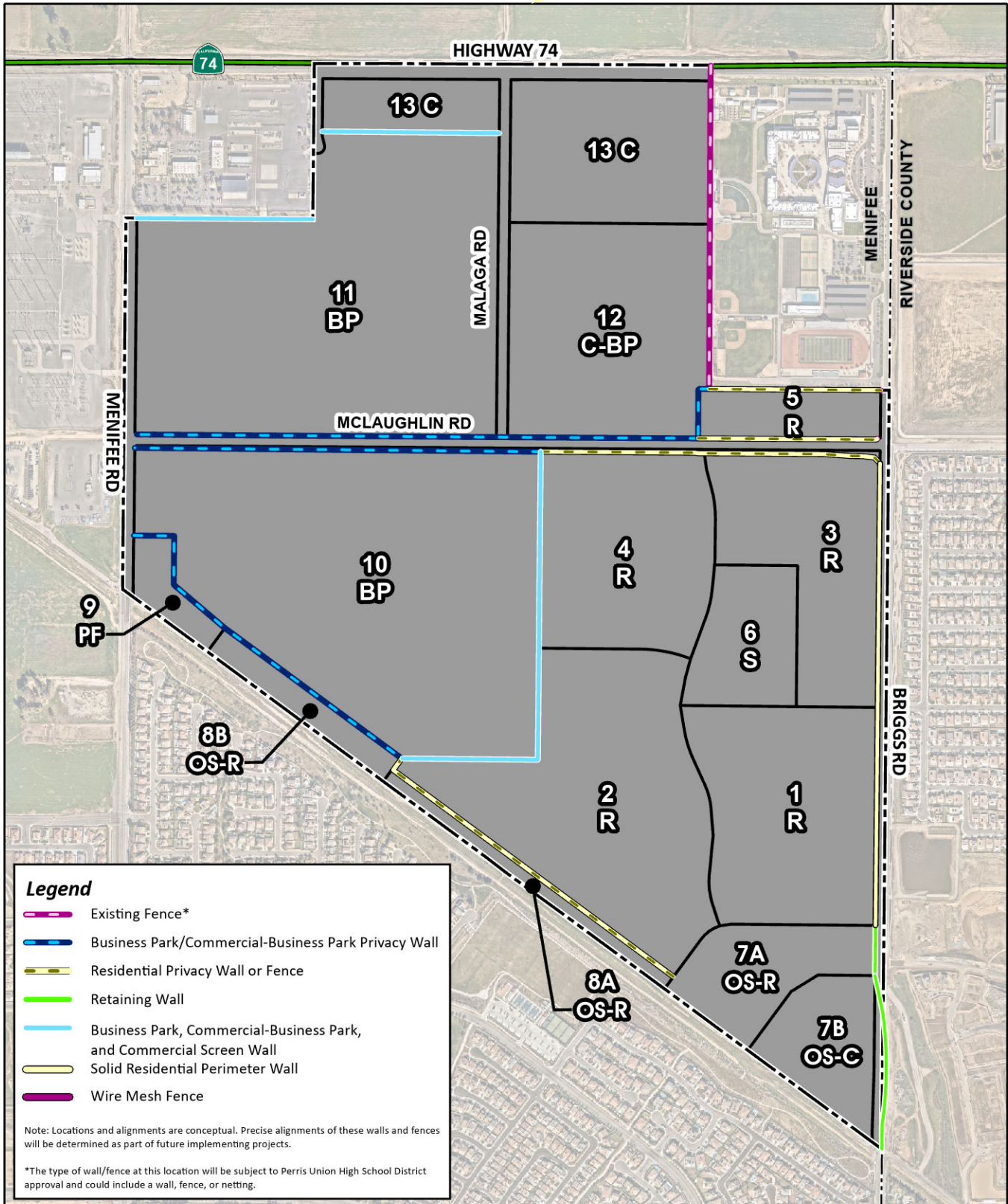
Four perimeter wall and fence types will occur in the community, which are described below. Refer to Figure 5-30, *Conceptual Wall and Fence Plan*, for expected locations of these walls and fences, and Figure 5-31, *Conceptual Wall and Fencing Types*, for conceptual examples. Not shown on Figure 5-30 are locations of interior residential lot side and rear walls and fencing, and fencing that may be required at the agri-commercial area, dog park, and other areas to enclose specific functions and uses. The locations and details of these fences will be determined at the time that improvement plans for the uses are brought forward.

Residential Perimeter Walls

These walls will define private and public spaces along Briggs Road and segments of the Residential Primary Entry Street, Residential Secondary Entry Streets, and the Residential Central Spine Street. Smooth finished, textured and/or colored masonry walls such as slump block will be used. Colors are anticipated to be earth tone and are to complement the abutting landscape treatment and any provided monument signage. Pilasters should be used to break up excessively long wall segments. Walls along public streets suggest providing columns or pilasters every 100 feet. In general, residential perimeter walls should be limited to a maximum of 6-feet-tall, unless additional height is necessary for noise attenuation, aesthetic relief from non-residential uses, retaining purposes, or other special circumstances.

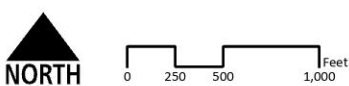
Business Park, Commercial-Business Park, and Commercial Screen Walls

These walls are provided within the Business Park, Commercial-Business Park, and Commercial areas and are primarily intended for security and aesthetic benefit. As a result, wall heights are anticipated to be as high as 14-feet in order to provide adequate and complete screening of truck courts and loading areas. To reduce the perceived wall height in adjacent residential areas, the finished building pad areas of the Business Park and Commercial Business Park uses can be designed to occur at a lower elevation than adjacent residential areas, thereby making the visible wall height lower as viewed from Residential planning areas. Alternatively, a landscaped berm can be used in combination with a wall to reduce the wall height. If these solutions are impractical, architectural relief applied on walls over 8-feet in height can be achieved by incorporating plane offsets, pilasters, textures, and/or other treatments to provide an aesthetically pleasing wall design. Walls not necessary for security or screening purposes should be limited to a height of 8-feet. Wall design facing public spaces, such as parks and streets adjacent to Business Park, Commercial-Business Park, and Commercial areas, are to provide finish detail such as pilasters and other features that match or complement the architectural articulation of the buildings. Perimeter walls along public streets are to provide columns or pilasters every 100 feet.

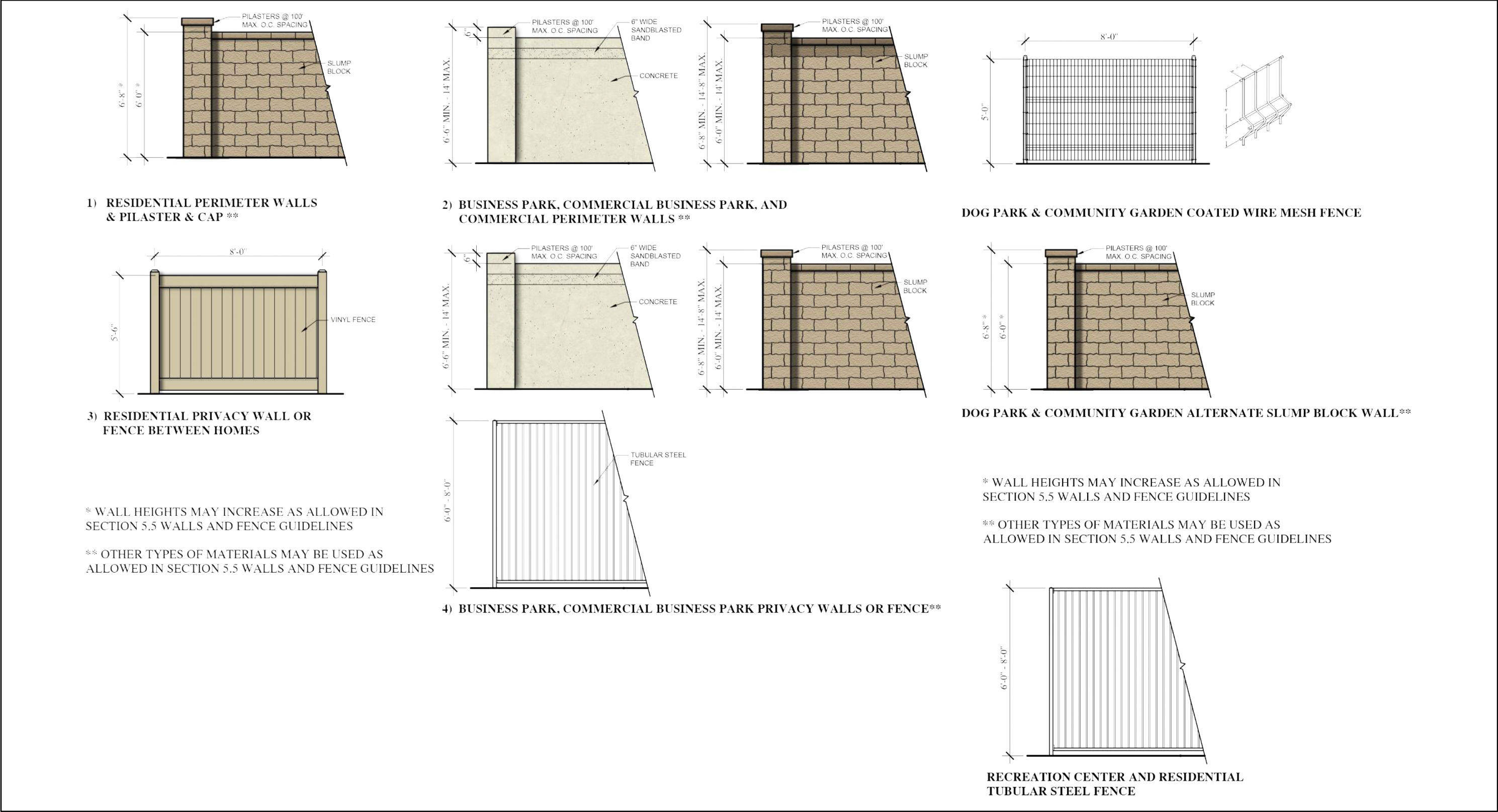


Source(s): ESRI, RCTLMA (2023), Nearmap Imagery (2023), Hunsaker & Associates (09-2021)

Figure 5-30



Conceptual Wall and Fence Plan



Source(s): Clark and Green (August 2023)

Figure 5-31

Residential Privacy Wall or Fence

There are some perimeters of Residential Planning Areas 1 through 5 that may or may not require a solid masonry wall, and instead would be better suited for a solid fence, wire mesh fence, or open fence. This includes the interface with off-site Heritage High School. Residential privacy walls and/or fences may also be established between residences and the community spaces, parks spaces, open space, and potential elementary school site. Wire mesh fencing may be established between the potential dog park and community garden permitted in Planning Area 5. Wall and fence height should be limited to 6-feet when abutting residential uses where privacy and noise are not an issue. Wall design facing public spaces shall provide finish detail such as pilasters at wall terminus points and use of wall caps, excluding perimeter walls along public streets which suggest providing columns or pilasters every 100 feet. Combination fence/walls are encouraged for walls placed between public spaces to encourage view opportunities, where appropriate.

Business Park and Commercial-Business Park Privacy Wall or Fence

There are some perimeters of Business Park and Commercial-Business Park Planning Areas 10, 11, and 12 that may or may not require a solid masonry wall, and instead would be better suited for a solid fence, an open fence, or combination wall and open fence. These areas include the interface with Planning Areas 8B and 9, McLaughlin Road, greenbelt along the southern MVSP property line, and civic center site, respectively. Wall and fence height should be no more than 8 feet. If more than 8 feet is needed, a wall should be used instead of a fence.

5.6 Public Art Guidelines

The Meniffee Valley area has a rich community history. Consistent with City of Meniffee General Plan policies, public art incorporating such history or that of the community shall be incorporated into select common public gathering areas such as in the Planning Areas 7A public park and in greenbelt areas near trails in Planning Areas 8A and 8B as well as along the trail that is designed to parallel the south side of McLaughlin Road. Public art may include but not be limited to interpretive plaques, murals, memorials, and sculptures.

5.7 Outdoor Lighting Guidelines

Outdoor lighting is an essential landscape architectural component that provides aesthetic appeal, enhances safe pedestrian and vehicular circulation, and adds to security. Lighting fixtures within the Specific Plan area shall comply with the following:

General Lighting Guidelines

- Lighting fixtures in the MVSP area should be from the same or complementary family of fixtures with respect to design, materials, fixture color, and light color. Use of LED lighting is encouraged.

- All exterior lighting designs should develop a sense of hierarchy by varying fixtures and illumination levels. Proper lighting helps to define the organization of streets and distinguishes vehicular and pedestrian circulation patterns. Entry areas (both pedestrian and vehicular), community facilities, and highly used recreation areas shall be creatively lit to develop a sense of place and arrival.
- Where desired, illuminate trees and other landscape features by concealed uplight fixtures.
- All outdoor lighting, including spotlights, floodlights, electrical reflectors and other means of illumination for signs, structures, landscape, parking, loading, unloading and similar areas shall be focused, directed, and arranged to minimize glare and illumination of streets or adjoining property. Low intensity, energy-conserving night lighting is preferred.
- Lights shall be of unbreakable plastic, recessed, or otherwise designed to reduce the problems associated with damage and replacement of fixtures. Fixtures shall be vandal resistant.
- Neon and similar types of lighting are prohibited, except when associated with retail commercial development in Planning Areas 12 and 13.
- All exterior lighting designs shall address the issue of security. Parking lots, pedestrian walkways, and building entrances shall be well lighted for security reasons.
- All exterior lights should be shielded where feasible and focused to minimize spill light into the night sky or adjacent properties.
- Locate all electrical meter pedestals and light switch/control equipment in areas with minimum public visibility or screen them with appropriate plant materials.

Street, Sidewalk, Trail, and Entry Monument Lights

- All public streets shall include street lighting in the public right-of-way that conforms to Caltrans (Highway 74) and City of Meniffee (Meniffee Road, Briggs Road, Malaga Road, and McLaughlin Road) public street standards for lighting and match the street light fixture style established on other segments of these roads beyond the MVSP.
- Public streets located interior to the MVSP shall have uniform lighting design standards regarding style, materials, and colors to ensure consistent design. Industrial collector roads serving the Business Park, Commercial-Business Park, and Commercial planning

areas may have different but complementary lighting fixture styles compared to the Residential planning areas.

- Along sidewalks, walkways, and trails, the use of low mounted fixtures (ground or bollard height) that encourage pedestrian scale, are encouraged.
- The lighting concept of the entry monumentation features is to illuminate the sign graphics and to gently wash the walls and pilasters with light. Concealed up light fixtures should illuminate trees and other landscape features.

Residential Community and Open Space-Recreation Lighting

- Freestanding lighting fixtures should not exceed a height of twenty-five feet (25') in the Residential and Recreation planning areas. Light poles necessary to illuminate sports fields in parks or at the elementary school site may exceed twenty-five feet (25') as approved by the City of Menifee.
- All community landscape common areas, including greenbelts, private recreation centers, streetscapes, parks, schools, the agri-commercial area and other like-areas may, at the discretion of the project developer or builders, contain accent lighting.

Business Park, Commercial-Business Park, Commercial and Public Facility - Civic Node Lighting

- Freestanding lighting fixtures should not exceed a height of thirty-five feet (35') in the Business Park, Commercial, Commercial-Business Park, and Public Facility - Civic Node planning areas.
- Illuminate parking lots, loading dock areas, pedestrian walkways, building entrances, and public sidewalks to the level necessary for building operation and security reasons.
- Truck courts and loading dock areas that are illuminated at night shall use the minimum intensity of light needed for safe and secure operations. Dimmers and motion sensor lights are encouraged to prevent these areas from being brightly lit when not in use.
- Minimize glare and "spill over" light onto public streets and adjacent properties by using downward-directed lights and/or cutoff devices on outdoor lighting fixtures, including spotlights, floodlights, electrical reflectors, and other means of illumination for signs, structures, parking, loading, unloading, and similar areas. Limit light spillover or trespass to one-quarter foot-candle or less, measured from within five feet of any adjacent property line.

Section 6

Architecture

Guidelines



Brookfield
Properties

6 – Architectural Guidelines

6.1 Purpose and Intent

The architectural design guidelines presented in this section provide a general framework for the quality and character of the built environment expected for the Meniffee Valley Specific Plan (MVSP). The guidelines are intended to ensure a consistent level of quality design that accommodates emerging architectural trends and supports green building practices. The Residential planning areas will incorporate a variety of home designs within a pedestrian-friendly environment. The Business Park and Commercial-Business Park planning areas will offer a contemporary employment and jobs center with building designs that incorporate attractive detailing from publicly-visible areas and use building articulation, window placement, and color changes for visual interest. The Commercial planning area will provide highly visible retail opportunities with building designs that promote visual compatibility and individuality within the MVSP. The Specific Plan area will be designed to embrace comfortable human scale, including at the pedestrian use areas of the Business Park, and well-proportioned spaces formed by architecture that is thoughtfully designed and detailed.

The architectural design guidelines contained herein are presented in a manner that ensures consistent architectural expression across the Specific Plan area, while allowing for flexibility in building design. The guidelines serve as guidance to developers, architects, engineers, builders, and property owners who will be involved in the development of the community. Architectural framework and direction are provided without limiting the creativity of the designer.

The Architectural Guidelines presented herein apply to all development within the MVSP. These guidelines may be subject to modification and contemporary interpretation to allow for responses to unanticipated conditions, including but not limited to changes in the real estate market, needs and desires of building users, technology advancements, and fluctuations in economic conditions.

6.2 Residential Architectural Design Guidelines

6.2.1 Design Guidelines

One of the goals of the MVSP is to allow for and integrate a variety of housing types into Residential Planning Areas 1, 2, 3, 4, 5, and 6, and to offer a range of choices in price and lifestyle for community residents. In other words, the product types in Planning Areas 1 through 6 can range from single-family homes to duplexes, to townhomes, to stacked flats and apartments and more, provided that the Development Standards given in Section 4 are followed and the landscape and architectural guidelines in Section 5 and this Section 6 are

met. In addition to providing housing type diversity, neighborhood design should also consider the connectivity within and between the neighborhood environment, how each neighborhood fits into the overall community, and visible edges of the neighborhoods. Key elements of the residential design theme include:

- A variety of housing opportunities for households of varying economic means, social needs, and life stages.
- Internal connectivity that enhances the relationship of buildings to the street and promotes walkability.
- Overall layout and design at a “human scale” with architectural diversity and pedestrian friendly streetscape.
- Strong visual and physical connections with parks and open space amenities.

The desired quality and character of the housing mix is shown below.



6.2.2 Residential Building Form and Typologies

The Residential planning areas are designed with flexibility to allow internal variety in residential densities and housing types. A wide range of residential building types and sizes can be integrated within the community. The mix of detached and attached building typologies have the following key attributes that add to diversity within the community.

Single-Family Detached and Cluster Units

Single-family Detached and Cluster Units provide a broad spectrum of home types. The ability to mix and match these home types allows visual interest and variety. The single-family residential unit types include conventional single-family, front-loaded single-family Z-lot, and cluster homes. These homes come in a wide range of configurations and unit sizes. These unit types are further described and illustrated below:

- *Conventional Single-Family Home*: This unit type will be plotted on a wide range of lot sizes and configurations. Access to the residences and garages are taken directly from the fronting street.
- *Front loaded single-family Z-lot Home*: This home type is designed to use a shared property line.. Reciprocal access and use easements are used to maximize side yard areas.
- *Cluster Home*: This single-family detached home type is “clustered” around a motor court or shared driveway. These units are typically in groups of 4 to 8 units. Unit entries either face the motor court/shared driveway or the street. Outdoor living space occurs in the side and rear yards. Reciprocal access and use easements are used to maximize side yard areas. Resident parking is provided in the garages with guest parking provided on adjacent streets or designated off-street parking areas.



Multi-Family Attached Units

Multi-family Attached residential product, including duplexes/triplexes, townhomes, and stacked flats, often resemble a small village, with the buildings generally oriented around public spaces, such as open space areas and recreational amenities. Buildings often form linear edges or green courts, creating opportunities for pedestrian connectivity.

Attached residential buildings consist of two or more dwelling units that share a common wall. There are a variety of configurations within this category of residential buildings, ranging from duplexes and townhomes to stacked units. Some examples are provided on the following pages, but other configurations of attached buildings are encouraged to provide diversity and a variety of housing choices, provided they meet the development standards of the underlying land use designations.

- **Duplexes and Triplexes:** Duplexes and triplexes are two and three individual dwelling units that are attached to each other. The dwellings' entries face the street or paseo. Private open space is provided in yards, patios, courtyards, or upper floor balconies.
- **Townhomes:** Townhomes are a collection of attached homes with shared walls and individual entries leading to a sidewalk, pedestrian path, or paseo. Private open space is provided in patios, courtyard, or upper floor balconies. Resident parking spaces are provided in garages, and guest parking spaces are provided on local streets or in designated parking areas. Garage access is typically provided via a motor court with shared driveways. When townhomes are combined with stacked flats or carriage units, they may include private rear yards and have entries facing the motor court.
- **Stacked Flats:** Stacked flats are attached multi-family dwellings with shared walls and individual unit entries accessed from sidewalks, pedestrian paths, or interior hallways or courtyards. The automobile access is via a private court drive. Resident parking spaces are provided in individual garages or designated onsite parking areas, and guest parking spaces are provided in designated parking areas and/or on local streets.



6.2.3 Residential Architectural Style

Residential architecture within the MVSP is intended to reflect themes and styles commonly found in southern California through the utilization of various forms, materials, and colors. Residential structures are intended to have a strong, coherent identity resulting in an aesthetically pleasing community.

Rather than specifically representing architectural styles, the development of architectural designs based upon styles historically used in this region is encouraged. A core architectural design goal of the Specific Plan is to achieve appropriate interpretations of the historical styles rather than exact recreations. Interpretations of selected architectural style shall address the economics of today's housing market and meet all current and applicable codes and standards within the building industry. Architectural styles (i.e. Cape Cod, Mediterranean/Spanish, Tuscan, Craftsman, Cottage, etc.) shall vary throughout Planning Areas 1, 2, 3, 4, 5, and 6, with individual character and interpretation encouraged. A selection of examples of the desired architectural styles were previously shown in Section 6.2.1.

Different elevations shall be required within each architectural style chosen. Elevation variations can be accomplished through the creative use of material and color palettes and different floor plans. Primary building colors shall provide a visually noticeable variation when the homes are grouped together along a street.



6.2.4 Residential Architectural Design Standards and Guidelines

The following is provided to ensure the creation of high quality, well designed residential community. Not only do these guidelines ensure a high level of quality in function and visual appearance, but they also encourage architectural character that creates variety and compatibility, thus enhancing the community's overall appeal and value.

Building and Garage Placement

- Orient buildings to face and frame the street to create a pedestrian friendly streetscape, as appropriate to the building typology.
- Give careful consideration to street orientation and building placement to help protect privacy, views, and visual quality of the neighborhood, as well as maximize solar access of the buildings where feasible and reasonable.
- Plot buildings to emphasize diversity and scale along the street and avoid visual monotony. Vary setbacks by using different plan forms and elevations on adjacent buildings, incorporating single-story elements, and utilizing different garage placements. Vary home types to increase diversity and promote a pedestrian-oriented street scene.
- Where feasible, arrange groups of attached residential buildings in clusters around outdoor spaces such as courtyards, pathways, and other gathering spaces and connections that encourage social activity and promote pedestrian connectivity.
- Incorporate a variety of garage placement options in the detached residential neighborhoods, including, but not limited to, front-loaded shallow-, mid- and deep-recessed garages, turn-in garages, split garages, tandem garages, and court-loaded garages. Garages should generally be set behind the front face of the building or be oriented to the motor courts. Roll-up doors are required for all garages.



In motor courts, landscape planting areas should be provided along court drives to soften the building appearance. Garage setbacks from the adjacent court drive must be three (3) feet when no full driveway is planned or greater than or equal to 18 feet when full driveways are provided. The minimum distance between the garage doors across the court drive shall be 30 feet. Guest parking spaces may be located between the buildings, in designated parking areas or along the street.

- Parking for attached residential buildings may be provided within attached private garages or in group parking areas.

Form and Massing

- Incorporate single-story elements such as porches, single-story living space, pop out gable elements, etc., where appropriate to the architectural style, to add variety to the street scene and establish pedestrian scale.
- Provide variation in floor plans, unit types, and roof forms to add visual interest to the neighborhood. For detached product types, each phase shall provide a minimum of three (3) floor plans, a minimum of three (3) elevations for each floor plan, and a minimum of four (4) different color schemes for each floor plan. For attached product types, each phase with up to 100 units, each builder product line must have a minimum of two (2) elevations and styles. Each attached product type with phases above 100 units shall provide an additional style for every additional 50 units. For attached product types, each elevation style shall provide two different exterior color schemes to ensure a variety of complementary building forms, massing, and colors within each neighborhood. A builder using the same marketing name and architectural products may build on contiguous blocks and nonadjacent blocks within Menifee Valley.
- Avoid flat, unarticulated walls and limit the bulk of the building elevations by providing vertical and/or horizontal staggers consistent with the architectural style of the building.
- Avoid long, massive multi-family buildings by limiting individual building lengths. This can be achieved by breaking buildings up into a collection of smaller, related buildings with paseos, courtyards, or similar spaces in between.
- Where multi-family buildings are located adjacent to single-family homes, provide a sensitive transition in scale, massing, and height, and design the transition to ensure resident privacy.

Articulation and Detail

- Building façades should be designed to provide an interesting connection between the public realm and private residence. Appropriate articulation and detailing include the following:
 - a. Variation in elevation styles
 - b. Variation in color schemes
 - c. Undulating building mass and roof planes
 - d. Vertical and horizontal stepped massing

- e. Visually reduced garages
 - f. Authentic architectural elements
 - g. Detailed entry features and openings such as doors, windows, porches, balconies, patios, courtyards, and trellises oriented toward the street
 - h. Vary wall planes to create depth and shadow, and avoid continuous, unrelieved walls surfaces along publicly visible elevations. Visual breaks can be created by a horizontal or vertical offset in the exterior wall plane, a recessed window or door, or other architectural detailing.
 - i. Select architectural details that are proportional, complementary, and authentic to the overall design of the elevation style.
- On corner lot buildings, consider wrapping pedestrian elements, such as porches and arcades, around the corners to provide human scale along both street frontages.
 - Side and rear elevations that are visible from public streets, parks, trails, and other highly visible areas should incorporate enhancement features that minimize visual monotony and enhance pedestrian experience, such as single-story massing at the exterior side, wrapped porches, window treatments, accent materials, pop-outs, insets, or other vertical or horizontal breaks in the wall massing.
 - Building elevations facing the motor courts should incorporate articulation such as massing offsets, projections or recesses, window detailing, etc. to improve the appearance of the motor courts.
 - Along motor court drives, enhanced paving should be provided within all or a portion of the court to soften the motor court appearance and complement the building architecture.

Materials and Colors

- Materials and colors should be consistent with the chosen architectural style and compatible with the character of surrounding development.
- Provide a variety in texture and color to allow for diversified expressions of individuality on building elevations, while maintaining visual cohesiveness throughout the community.
- Ensure that materials and color blocking terminate at inside corners or is otherwise wrapped to avoid the appearance of false façades.

- Apply colors and materials to enhance changes in wall plane, reinforce articulation of elevations, and enhance special features such as entries, single-story elements, etc.
- Materials should be consistently applied and work harmoniously with adjacent materials. Avoid piecemeal embellishments and frequent changes in materials.
- Select high-quality, low-maintenance, and durable materials to minimize the need for replacement that would contribute to landfill waste.

Functional Elements

- Work with service providers to minimize visual clutter and eliminate above ground location conflicts with utility hardware. Techniques to be considered include undergrounding where possible, landscape or material screening, and use of neutral or complementary colors.
- Gas and electrical meters should be placed in utility cabinets or otherwise screened to be integral with the architecture of the building.
- Roof-mounted and ground-mounted mechanical equipment such as air conditioning/heating equipment, pool/spa equipment, etc. (excluding solar panels) should be screened from view of streets, paseos, and other public spaces.
- Mechanical devices such as exhaust fans, vents, and pipes shall be painted to match the colors of the surfaces to which they are attached.
- Exposed gutters and downspouts must be colored to match or complement the surface to which they are attached.
- Where trash and recycling material containers are provided to individual units, space should be provided in a side or rear yard or in the interior of the garage to accommodate a minimum of two collection containers.
- Where trash and recycling material collection facilities are shared by several units/buildings, the collection facilities should be screened by architectural enclosures. The screening enclosure materials and colors should be similar or complementary to the exterior materials and colors used on the adjacent principal buildings.
- Trash and recycling material collection areas should be sited for convenient access but should avoid impacting important neighborhood features such as entries, recreation

areas, leasing offices, and clubhouses. Trash and recycling material collection facilities for multi-family residential complexes should not be located adjacent to a public street.

- Exterior light fixtures should be designed to complement the architectural style of the building. Exterior lighting should be shielded to conceal the light source and minimize glare and light spill to adjacent properties and public rights-of-way.
- Multi-family residential complexes shall be sufficiently lighted to ensure night-time mobility and deter criminal activity. Choose lighting locations for maximum visual enhancement and safety.

6.3 Business Park Architectural Design Guidelines

6.3.1 Design Theme

The architectural style of the Business Park Planning Areas 10 and 11 emphasizes building massing over structural articulation. Buildings are characterized by simple and distinct cubic masses with interlocking volumes of wall planes, colors, and materials to create visual appeal. Exterior building colors are light and gray tones with stone, glass, or steel materials at focal points, such as around building entrances and near outdoor gathering spaces. Additionally, architectural designs may mix colors, materials, and textures to articulate façades and create visual appeal.

Design elements are selected to be compatible in character, massing, and materials in order to promote a clean and contemporary feel. Individual creativity and identity are encouraged, but design integrity and compatibility must be maintained among all buildings and planning areas to reinforce a unified image and campus-like setting.

The desired quality and character of Business Park buildings is shown below.





6.3.2 Business Park Architectural Design Standards and Guidelines

The following is provided to ensure the development of a Business Park that emphasizes a contemporary interpretation of the traditional architectural styles found in the surrounding area. Design elements are selected to be compatible in character, massing, and materials in order to promote a clean and contemporary feel, yet individual creativity and identity is encouraged from building to building.

Building Placement and Orientation

The following guidelines apply to Business Park buildings within the MVSP area to ensure that development is visually consistent, appealing, and inviting. Note that building faces that orient inward to truck courts or service areas and that are not visible from public roads, or publicly accessible viewing areas, are not required to adhere to the below building form guidelines. In instances where the tops of the building are visible from public roads or public viewing areas, the building would also be required to adhere to the below building form guidelines.

- Use simple geometric shapes as the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Avoid arbitrary, complicated building forms.
- Long horizontal wall planes visible from a public street should include periodic changes in exterior building materials, color, decorative accents, and/or articulated features.
- Modulation and variation of building masses and wall planes between adjacent buildings visible from public streets is encouraged.
- Locate the office portion of warehouse buildings at the corner(s) of the building. For buildings adjacent to Menifee Road or Highway 74, orient the office toward these roads to provide visual interest from the public street.
- Make pedestrian entrances to buildings (with the exception of service doors and emergency exit doors) obvious through changes in massing, color, and/or building materials.
- Pedestrian and ground-level building entries intended for visitor use should be recessed or covered by architectural projections, roofs, or arcades in order to provide shade and visual relief.
- Business Park buildings should include an outdoor employee amenity area, including tables and chairs so that workers do not have to travel off-site for outdoor enjoyment.

- Architectural and trim detailing on building façades shall be clean, simplistic, and not overly complicated.
- Materials applied to any elevations shall turn the corner of the building to a logical termination point in relation to architectural features or massing.
- Trailer truck and automobile vehicle access drives and parking shall be separated.

Building Materials, Colors and Textures

Building materials and colors play a key role in developing a clean, contemporary visual environment; therefore, the selected exterior materials, colors, and textures should complement one another throughout the Business Park Planning Areas 10 and 11. Slight variations are encouraged to provide visual interest.

- Appropriate primary exterior building materials include concrete and similar materials, as well as tilt-up panels. The primary materials should be accented by secondary materials including but not limited to natural or fabricated stone, fire resistant wood siding (horizontal or vertical), and metal.
- Trim details may include metal finished in a consistent color, plaster, or concrete elements finished consistently with the building treatment. Use of overly extraneous “themed” detailing, like oversized or excessive foam cornice caps, foam molding and window detailing is discouraged. Material changes should occur at intersecting planes, preferably at the inside corners of change of wall planes, or where architectural elements intersect.
- Primary exterior building colors should be light and gray tones. Darker and/or more vibrant accent colors may be provided in focal point areas, such as around building entrances and near outdoor gathering spaces.
- Bright primary colors, garish use of color, and arbitrary patterns or stripes that will clash with this color palette are discouraged, except in signage logos.



- Exposed downspouts (only permitted if not in public view), service doors and mechanical screen colors shall be the same color as the adjacent wall.
- If downspouts are needed in areas of public view, they shall be designed as internal downspouts

Windows and Doors

- The patterns of window and door openings shall correspond with the overall rhythm of the building and should be consistent in form, pattern, and color. Window trims shall be finished in a consistent color on each building.
- When possible, the positioning of doors and windows on individual building façades should occur in a symmetrical and repetitive pattern to create continuity.
- Glass shall be clear or colored with subtle reflectiveness. Highly-reflective glass is prohibited.
- Pedestrian entry doors to buildings shall be clearly defined by features such as overhangs, awnings, and canopies or embellished with decorative framing treatments – including but not limited to accent trim. Dark and confined entries, flush doorways (except emergency exit and service doors) and tacked-on entry alcoves are discouraged.



Truck Court and Loading Dock Design

- Loading doors, service docks, and equipment areas should be oriented or screened to reduce visibility from public roads and publicly accessible locations. Screening may be accomplished with solid walls or fences that are compatible with the architectural expression of the building. Screening may also be accomplished by a combination of landscaping and solid walls or fences.
- Buildings located adjacent to Heritage High School shall not have loading docks on building façade(s) facing the school campus.
- No loading or unloading activity is permitted to take place from public streets/view.

- An adequate queuing distance of a minimum of 150 feet should be provided on-site in front of security gates to avoid the circumstance of trucks stacking on public streets waiting to enter at entry gates.
- Truck and service vehicle entries should be designed to provide clear and convenient access to truck courts and loading areas such that passenger vehicle, pedestrian, and bicycle circulation is not adversely affected by truck movements.
- Loading bays that are utilized by refrigerated trailers shall be equipped with plug-in electrical outlets.
- Conduit should be installed in truck courts in logical locations that would allow for the future installation of charging stations for electric trucks.

Equipment Screening

- Ground-mounted equipment, including but not limited to mechanical or electrical equipment, emergency generators, boilers, storage tanks, risers, and electrical conduits, should be screened from public viewing areas including adjacent public roads. Screening may be accomplished with solid walls, fencing, or landscaping.
- Electrical equipment rooms should be located within the building envelope. Pop-outs or shed-like additions are discouraged.
- Wall-mounted items, such as electrical panels, should not be located on the building façade facing adjacent public roads/views. Wall-mounted items should be screened or incorporated into the architectural elements of the building so as not to be visually apparent from the street or other public areas.
- Rooftop equipment, including but not limited to mechanical equipment, electrical equipment, storage tanks, wireless telecommunication facilities, satellite dishes, vents, exhaust fans, smoke hatches, and mechanical ducts, shall be screened by rooftop screens or parapet walls so as not to be visible by the public.
- Integrate rooftop screens (i.e., parapet walls) into the architecture of the main building. Wood finished rooftop screens are prohibited.
- Design the roofs of Business Park buildings to support the future installation of solar panels.



- Roof access (via roof ladders or other means) must be located interior to the building.

Trash Enclosures

- All outdoor refuse containers shall be screened within a permanent, lockable, and durable enclosure and should be oriented to avoid visibility from public roads/views. Enclosure design shall reflect the architectural style of adjacent buildings and use similar, high-quality materials.
- All outdoor trash enclosures shall be constructed with solid roofs to prevent exposure of dumpster contents to rainfall and prevent polluted stormwater runoff from these structures.
- Refuse collection areas shall be located behind or to the side of buildings, away from the building's main entrance and public view.
- Buildings shall be designed to meet all Waste Department requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.

6.4 Commercial-Business Park and Commercial Design Guidelines

Buildings in Commercial-Business Park Planning Area 12 and Commercial Planning Area 13 will contain smaller buildings and buildings that are more visitor-serving. The design objective is to focus architectural articulation and detailing toward the portions of building elevations that are highly visible to the public, while promoting a pedestrian oriented site design that incorporates pedestrian amenities, outdoor spaces, and physical and visual connections as appropriate to the use of the buildings. The desired quality and character of Commercial-Business Park and Commercial buildings is shown below.



Building Form, Placement and Orientation

- Visual prominence from streets and public spaces should be the primary consideration when determining the building location and design. Massing elements should be used to create focal points at street corners and view termini.
- Buildings located adjacent to Highway 74 should face Highway 74. Where this orientation is not possible, the building façade facing Highway 74 should be highly articulated to appear as a front façade.
- The arrangement of buildings, parking, drive aisles, and common areas should consider the anticipated building functions and activities and relate to the surrounding built environment in pattern, function, scale, character and materials.
- Locate buildings and on-site circulation systems to minimize pedestrian/vehicle conflicts. Link structures to the public sidewalks, where possible, with walkways and landscaping. Where appropriate, incorporate enhanced treatments such as decorative paving and at key pedestrian crossings and spaces to support a pedestrian-friendly environment.
- Where possible, arrange buildings to create outdoor spaces that incorporate pedestrian amenities such as plazas, shades, seating and fountains, as appropriate to the use of the site.
- Primary pedestrian entries should be a main feature of the building elevation. Use recesses, projections, columns and other distinctive architectural elements to articulate entries so they are clearly identifiable.
- Buildings should not appear bulky. Create visual breaks by using such techniques as varying the height of a building or articulating the building façade through horizontal and vertical offsets in wall planes.
- Use massing, façade articulation, roof forms, and architectural detailing to integrate the scale of all buildings within the planning area. Vertical architectural elements may be used as focal points to identify primary structures or locations.
- The size of windows and doors and associated structural detailing should relate to the scale of the elevation on which they appear.

- For buildings that accommodate commercial retail uses, arcades, trellises, and awnings are recommended for functional as well as aesthetic reasons. Awnings located on a row of contiguous structures should be coordinated with regard to location, form, and color.

Building Materials, Colors and Textures

- Appropriate primary exterior building materials include concrete and similar materials, as well as tilt-up panels. The primary materials should be accented by secondary materials including but not limited to natural or fabricated stone, fire resistant wood siding (horizontal or vertical), and metal.
- Exterior building materials should be durable and require low maintenance.
- Ensure that materials and color blocking terminate at inside corner or is otherwise wrapped to avoid the appearance of false façades.
- Primary colors should be earth-toned or gray, with accent colors at entries and focal points. Variations in shade or tone can be used to enhance form and heighten interest.

Loading Docks and Equipment Screening

- Mechanical and utility equipment should be placed in locations which are not exposed to direct view from the street or be suitably screened. The screening device materials should be compatible with the materials and colors of the adjacent principal buildings.
- Roof-mounted equipment should not be visible from streets, parks, and other public spaces. The screening of roof-mounted equipment should be incorporated into the design of the roof. Full parapets, sloping roofs, and low parapets with supplemental screens may be used to screen roof equipment, provided that roof-mounted screen walls are fully integrated with the building's architecture.
- Loading, service, and storage areas should be located away from activity areas and be screened from public view with landscaping and/or walls.
- Outdoor trash and recycling material collection areas should be completely enclosed and screened from view by a wall or fence. All such areas should have concrete floors and loading pads and be of sufficient size to accommodate all site generated refuse.
- Fences and walls that provide screening should be designed as an integral part of the building design concept and be constructed of materials which are complementary in color, finish, and texture to the adjacent principal buildings.

6.5 Agri-Commercial Uses Design Guidelines

Agri-Commercial activity is anticipated within Planning Areas 5 and 13. This activity is intended to support small scale farming activity focused upon crops and animal husbandry operations combined with related commercial activity. Design guidelines for this use are as follows:

- Primary building structures should be designed to be representative of and complimentary to past historical barns and support structures of the Meniffee Valley community and region.
- Outbuildings, sheds, and fencing should have design, materials, and color consistency with the primary building structures.
- Commercial buildings (stores, restaurants, etc.) should have design, materials, and color consistency with the building structures while also encouraging and accommodating space for outdoor dining, outdoor displays of produce and products for sale, as well as space for special events and activities.
- The design of temporary structures should be in keeping with the primary building and similar structures of the Meniffee Valley community and region.



6.6 Public Facility - Civic Node Design Guidelines


The Public Facility - Civic Node in Planning Area 9 will contain smaller buildings than the MVSP's Business Park buildings. The design objective is to focus architectural articulation and detailing toward the portions of building elevations that are highly visible to the public, while promoting a pedestrian oriented site design that incorporates pedestrian amenities, outdoor spaces, and physical and visual connections as appropriate to the use of the buildings.

Public Facility - Civic Node buildings should emulate the grouping of styles outlined in Section 6.4, *Commercial Business Park and Commercial Design Guidelines*. Design guidelines for this use are as follows:

- Primary building structures should be designed to be representative of and complimentary to support structures of the Menifee Valley community and region.
- The arrangement of buildings, parking, drive aisles, and common areas should consider the anticipated building functions and activities and relate to the surrounding built environment in pattern, function, scale, character and materials.
- Locate buildings and on-site circulation systems to minimize pedestrian/vehicle conflicts. Link structures to the public sidewalks, where possible, with walkways and landscaping.

Section 7

Administration and Implementation



Brookfield
Properties

7 – Administration and Implementation

This section sets forth the policies and procedures for the City of Menifee’s review and approval of implementing development projects within the Specific Plan area and describes the methods and procedures for interpreting and amending the Specific Plan as necessary.

All development within the community is subject to the implementation procedures described in this section. Additional information on implementation, including potential funding mechanisms, maintenance responsibilities, and monitoring activities, are also presented herein.

7.1. General Administration

Pursuant to Government Code Section 65451, all specific plans must contain a “program of implementation measures including regulations, programs, public works projects, and financing measures” necessary to implement the specific plan. This section defines the administration of the Menifee Valley Specific Plan (MVSP) and the implementation process for approving new development, including the accompanying financing, phasing, and other necessary programs.

Applicability

Approval of the MVSP indicates acceptance by the City of Menifee City Council of a general framework for the development of MVSP area. Part of that framework establishes specific development standards that constitute the zoning regulations for the Specific Plan (refer to Section 4, *Development Standards*). All development and land uses in the Specific Plan shall comply with the requirements and standards set forth in this Specific Plan. Where conflicts exist between the standards set forth in this Specific Plan and those found in Menifee Zoning Code, the standards in the Specific Plan shall apply. Standards not addressed in this Specific Plan are subject to the Menifee Zoning Code.

Severability

This Specific Plan document enables the City of Menifee to facilitate the processing and approval of development plans and implementing permits to build out the MVSP area. If any section, regulation, sentence, clause or phrase of this Specific Plan or future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Plan.

Interpretation

In the event that there are conflicts between the Specific Plan regulations and the provisions of the City of Menifee Zoning Code, the provisions of the MVSP shall prevail. If there is a question or

ambiguity regarding the interpretation of any provision of this Specific Plan, the Community Development Director has the authority to interpret the intent of the provision, using the spirit and intent of the Specific Plan as a guide.

The Community Development Director may, at his/her discretion, refer interpretations to the Planning Commission for consideration and action. Such a referral shall be accompanied by a written analysis of issues related to the interpretation. All interpretations made by the Community Development Director and decisions of the Planning Commission may be appealed per applicable provisions of the City's Zoning Code.

7.2. Review Process for Implementing Development

The MVSP will be implemented through the processing of numerous discretionary entitlements and ministerial actions. The implementation process provides the mechanism for reviewing development plans and ensuring development consistency with the Specific Plan's objectives. The City of Menifee shall be responsible for the administration and enforcement of the MVSP in accordance with the provisions of this Specific Plan, the State of California Government Code, and the Subdivision Map Act, including: processing assistance, interpretations of provisions, approval of administrative permits, issuance of permits, site development plans, approval of temporary or interim uses, specification of conditions of approval, and authorization of certificates of occupancy for new development.

The City Manager (or their designee) shall have final approval, conditional approval, or denial of development proposals that are consistent with the Specific Plan and all development and design standards and shall have the authority to determine if Planning Commission and/or City Council review would be required. Development proposals consistent with the Specific Plan and all development and design standards include: residential tract maps, commercial plot plans, and industrial plot plans if buildings do not exceed 1,450,000 s.f. in Planning Area 10, 1,400,000 s.f. in Planning Area 11, and 570,000 s.f. in Planning Area 12. The City's decision appeal procedures shall continue to apply to development proposals consistent with the Specific Plan. Any proposal that does not meet the criteria listed above shall be reviewed by the Planning Commission and/or City Council as discussed below. The administrative review process described above shall only apply during the effective term of the Development Agreement. If the Development Agreement effective term has lapsed, the standard review process as stated in the City's Development Code would apply.

The Planning Commission shall have final approval, conditional approval, or denial of conditional use permits, and public use permits and is authorized to approve /certify the appropriate CEQA document. The Planning Commission shall be responsible for recommending approval, conditional approval, or denial to the City Council regarding any general plan amendments, specific plan amendments, and zoning ordinance or zone changes. Further, the Planning Commission shall act on appeals from decisions by the Community Development Director.

Review Process for Implementing Residential Development

A variety of residential product types are expected to be developed in Residential Planning Areas 1-5 to establish a diverse and sustainable residential community. Residential development is also permitted in Planning Area 6 should a school not be developed in Planning Area 6. The number, type, and location of residential lots and dwelling units (as selected from the LMDR, MDR, MHDR, and HDR categories as described in Section 4, *Development Standards*) will occur at the tentative tract map stage of the development process. The unit types (LMDR, MDR, MHDR, and HDR) and the target number of residential units for each Planning Area may vary from those set forth within this Specific Plan as presented in Table 2A, *Detailed Land Use Summary* and Table 4B, *Planning Areas 1, 2, 3, 4, and 5 Development Standards*. Modifications can occur in conformance with the following standards:

1. The total number of residential dwelling units in the MVSP shall not exceed 1,718 units.
2. The development of fewer residential units in any Planning Area below the target number of dwelling units or expected density range specified for that Planning Area in Table 2A is permitted and may occur without need of a substantial conformance review or amendment to this Specific Plan. However, in no case shall the density of any implementing residential development be less than 4 dwelling units per acre (du/ac).
3. The development of more residential units in Planning Areas 1-6 above the target number of dwelling units or expected density range specified for that Planning Area in Table 2A is permitted and may occur up to a 20% increase without need of a substantial conformance review or amendment to this Specific Plan. In the event the school site is developed in Planning Area 6, the units allocated to Planning Area 6 may be reallocated to Planning Areas 1-5. However, in no case shall the density of any implementing residential development exceed 24 dwelling units per acre (du/ac). Furthermore, in no case shall the maximum unit count of the Specific Plan (1,718 dwelling units) be exceeded.
4. The unit types (LMDR, MDR, MHDR, and HDR) expected in each Planning Area as indicated in Table 4B are permitted in any of the residential Planning Areas 1-5 and the school site Planning Area 6 should a school not be developed, provided that the development standards for the unit type given in Table 4B are followed and the placement of the unit type is compatible with the character of any existing, adjacent residential development that has already established in the MVSP.

Review Process for Implementing Business Park and Commercial Development

A variety of uses are expected to be developed in Business Park Planning Areas 10 and 11, Commercial Business Park Planning Area 12, and Commercial Planning Area 13. The number, type,

and location of buildings will be determined at the tentative tract map and plot plan approval stages of the development process.

1. Development in the Business Park and Commercial Business Park Planning Areas 10, 11, and 12 shall not exceed 5,510,00 square feet of building space combined.
2. The development of more building space in Planning Areas 10, 11, 12 above the target amount of building space specified for that Planning Area in Table 2A is permitted and may occur up to a 30% increase without need of a substantial conformance review or amendment to this Specific Plan, provided that the combined total square feet does not exceed 5,510,000 square feet and the FAR for any individual parcel does not exceed 0.60 FAR.
3. For commercial uses developed in Planning Area 12, the commercial building space shall count as double the square footage against the maximum allowed square footage in Planning Area 12. For example, if a 50,000 square foot commercial building is implemented, it shall count as 100,000 square feet of the maximum building square footage permitted in Planning Area 12.
4. Development in Commercial Planning Area 13 shall not exceed 560,000 square feet of commercial building space. If Planning Area 13 is developed with less than 560,000 square feet of commercial building space, the remaining space may be transferred into Planning Area 12 without need of a substantial conformance review or amendment to this Specific Plan.
5. The development of a lesser amount of building space in Planning Areas 10, 11, 12, or 13 below the target amount of building space specified for that Planning Area in Table 2A is permitted and may occur without need of a substantial conformance review or amendment to this Specific Plan.

7.3. Specific Plan Conformance Determinations and Specific Plan Amendments

Modifications to this Specific Plan's text and exhibits may be necessary to accommodate proposed implementing development projects. Changes to the adopted Specific Plan shall be classified by the Community Development Director as either a Substantial Conformance Determination or Specific Plan Amendment. The applicant shall submit a detailed justification explaining why a Substantial Conformance Determination or Specific Plan Amendment is warranted and any application or necessary fees exhibits deemed necessary by the Community Development Director.

Substantial Conformance

A Substantial Conformance application may be approved by the Community Development Director with input from relevant departments. Substantial conformance allows for the administrative

approval and interpretation of minor modifications to the Specific Plan text, graphics, and/or project design that do not change the meaning or intent of the Specific Plan. Through the review and approval process, an implementing project may be found in substantial conformance with the provisions of this Specific Plan and may be approved, conditionally approved, or denied by the Director under the circumstances listed below. The Community Development Director shall also have the discretion to refer any such request for substantial conformance to the Planning Commission for interpretation and action. Items of Substantial Conformance include, but not limited to the following:

- Simple edits or clarifications to text, graphics, or figures that do not change the meaning or intent of the Specific Plan.
- A modification of the project design including minor adjustments to Planning Area boundaries that does not alter the intent of the Specific Plan, that improves circulation, protects topographic features, minimizes grading, improves drainage, or improves infrastructure.
- Shifts in internal road right-of-way alignments, widths, streetscape amenities, and access points that would not substantially alter the land use or circulation system set forth in this Specific Plan with approval from the Director of Public Works.
- Changes to the locations and sizes of infrastructure systems, including drainage, grading, water, and wastewater plans that would not substantially alter the plans set forth in this Specific Plan, provided the changes can be supported by technical studies reviewed and approved by the city.
- Modifications of design elements such as paving treatment, colors, architectural details, signs, landscape, fencing, lighting, and entry treatments as long as the Community Development Director finds the change to be compatible with the intent of the Specific Plan and/or previous developments/approvals in the MVSP area.
- Changes to the Phasing Plan provided that the Community Development Director determines that infrastructure is available and constructed to serve that phase and that any mitigation measures linked to that phase, location, or level of development are implemented.
- Increases in the number of residential units in any Planning Area that is more than 20% of the target number of dwelling units specified for that Planning Area in Table 2A provided that the density of the development does not exceed 24 dwelling units per acre (du/ac) and provided that shifts in the number of dwelling units between planning areas do not increase the maximum overall number of dwelling units (1,718) in the Specific Plan.

- In the event the school site is developed in Planning Area 6, the reallocation of dwelling units from Planning Area 6 to Planning Areas 1-5 provided that the above conditions related to increases in the number of residential units are met.
- Increases in the amount of business park square footage in any Planning Area that is more than 30% of the target amount of square footage for that Planning Area in Table 2A provided that the total amount of business park building space does not exceed 5,510,000 square feet.
- A land use that is not specifically discussed in this Specific Plan but that is similar in nature to those listed as allowable in the Specific Plan and does not increase intensity in any phase or Planning Area.
- Any changes to the Specific Plan that are required to respond to permit requirements of applicable state or federal agencies with jurisdiction over the Specific Plan will be deemed in substantial conformance with the Specific Plan and will not require an amendment so long as the purpose of such changes is the protection of natural resources.

Specific Plan Amendments

Amendments as defined in this Specific Plan, shall be processed according to the provisions of the City's Zoning Code. An amendment, as defined in this Specific Plan, is any of the following:

- Changes to exhibits or text that alter the intent of the Specific Plan.
- Changes to development standards and/or design guidelines, which, if adopted, would substantially change the physical character of the Specific Plan.
- An increase in the permitted density of residential development above 24 dwelling units per acre (du/ac).
- A new type of land use that is not specifically discussed in this Specific Plan and that is not of the same intensity and character as those listed in this Specific Plan.
- Any change that would trigger the preparation of a Supplemental EIR.
- Changes in land use boundaries that result in an increase of more than the maximum allowable development potential, as analyzed in the certified EIR prepared for the MVSP.

7.4. Phasing Plan

The MVSP area is expected to be mass graded in one phase, with fine grading and implementing development to follow in three phases. Implementing Residential development is expected to commence in the southern portion of the MVSP area and move northerly in sequence. Implementing Business Park and Commercial Business Park development is expected to commence in the northern

portion of the MVSP area and move southerly in sequence. Commercial development along Highway 74 is anticipated as the third and final phase of development.

Given the size of the property, grading and implementing development activities in the MVSP area may occur concurrently. Various land uses may be under development at the same time and built-in response to market demands and according to a logical and orderly extension of roadways, public utilities, and infrastructure. Planning Areas may be developed out of the expected sequence, or in smaller increments, provided the required infrastructure and services are available at the time of development to provide adequate access to and from the MVSP community and ensure public health and safety.

Figure 7-1, *Conceptual Phasing Plan*, provides a conceptual phasing plan. Table 7A, *Open Space Phasing Thresholds*, provides conceptual timing thresholds for the open space planning areas. The exact timing of implementation and phasing for any Planning Area may vary based on a number of factors, including market and economic demands, as well as physical constraints or timing of infrastructure improvements. The final phasing for the development will be determined through future implementing projects and any phasing requirements specified in a Development Agreement that accompanies this Specific Plan.

The expected development sequence is as follows:

I. Phase 1:

- a. Planning Areas 1 and 2 (Residential)
- b. Planning Areas 7A, 7B, and 8A (Open Space- Recreation, Open Space- Conservation, and Greenbelts)
- c. Planning Area 11 (Business Park)
- d. Planning Area 12 (Commercial Business Park)
- e. Residential Spine Street; Briggs Road (along Planning Areas 1, 7A, and 7B); Menifee Road (along Planning Area 11); McLaughlin Road; and Malaga Road (from Highway 74)

II. Phase 2:

- a. Planning Areas 3, 4, 5 (Residential)
- b. Planning Area 6 (School)
- c. Planning Area 8B (Open Space-Greenbelt)
- d. Planning Area 10 (Business Park)
- e. Briggs Road (along Planning Areas 3 and 5) and Menifee Road (along Planning Area 10)

III. Phase 3:

- a. Planning Area 9 (Public Facility)
- b. Planning Area 13 (Commercial Retail)
- c. Highway 74 and Menifee Road (Along Planning Area 9)
- d. Pedestrian and Bike-Only Bridge

Table 7A - Open Space Phasing Thresholds

PLANNING AREA	TIMING
7A (<i>Open Space - Recreation</i>)	Prior to 275 th Residential Building Permit, the property must be irrevocably offered for dedication to the City and the park plot plan approved by the City. Prior to 371 st Residential Building Permit, park shall be constructed and fully operational. The park is required to be under construction prior to issuance of the first Business Park Building Permit.
7B (<i>Open Space - Conservation</i>)	Not applicable to conservation land.
8A (<i>Open Space - Recreation</i>)	The open space area is required to be constructed prior to the first Residential Certificate of Occupancy.
8B (<i>Open Space - Recreation</i>)	The open space area is required to be constructed prior to issuance of the first Business Park Building Permit in Planning Area 10.
<i>Paseos Between Business Park and Residential Uses</i>	Prior to issuance of any residential building permit within Planning Areas 2 or 4, any proposed paseo or landscape setback area adjacent to Planning Area 10 shall be complete. Furthermore, any proposed paseo or landscape setback area adjacent to Planning Area 10 shall be complete.

7.5. Financing Plan

Various techniques are available for financing the required improvements for the MVSP. A detailed financing plan should be prepared in order to successfully implement the improvements and programs outlined by the Specific Plan. Along with establishing specific goals and policies, the financing plan should analyze a series of methods to finance infrastructure and other improvements, recommend preferred alternatives, and develop a process for enacting financing methods.

The appropriate mechanism for implementing each improvement shall be tied to the construction schedule and established conditions of approval for implementing projects. The following is a summary of possible methods that could be used to finance Specific Plan improvements. There may be other sources available to finance improvement projects, such as special assessment districts, government grants, or various types of bonds not listed below.

Developer Funding

Certain onsite facilities and improvements are directly associated with various phases of development. It is reasonable to expect the developer, builder or property owner to pay all or a majority of the costs associated with these facilities and improvements in order to secure development rights. Onsite local streets, utility connections from main trunk/service lines, and drainage facilities are examples of facilities and improvements typically required concurrent with development and often funded by the developer or builder.

Special Assessment Districts

A special assessment district is a type of benefit district that requires a vote by the property owners to encompass a defined and limited geographic area. The city or other agencies may form collectively or individually, one or more special assessment district under one of several different statutory acts to construct public improvements, such as streets, storm drains, sidewalks, streetlights,

sewer facilities, parks landscape, and other similar capital facilities. The special assessment districts can issue bonds to finance those improvements and levy a special assessment to pay debt service on those bonds.

A special assessment district may fund improvements within the entire Specific Plan area or smaller areas in the Specific Plan where special improvements are constructed that directly benefit only certain property owners. Special assessment districts may only be used to pay for projects that are of specific and direct benefit to the property owner being assessed. The amount of the assessment must directly relate to the amount of benefit received by the property owner.

Landscaping and Lighting Districts (LLD)

Landscaping and Lighting Districts (LLD) may be used for maintenance and servicing of landscaping and lighting through annual assessments on benefiting properties. LLDs may also provide for maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities.

The Specific Plan area is currently within the boundaries of the Landscaping and Lighting Maintenance District 89-1C (L&LMD 89-1C) and County Service Area (CSA) 145. However, the MVSP area will be detached from L&LMD 89-1C and may annex into a maintenance CFD in lieu of the L&LMD to provide maintenance services to certain approved public improvements.

Community Facilities Districts (CFD)

The Mello-Roos Community Facilities Act of 1982 allows the creation of special districts authorized to levy a special tax and issue tax exempt bonds to finance public facilities and services. A community facilities district may be initiated by the legislative body or by property owner petition and must be approved by a 2/3 majority of property owners or registered voters (if there are more than 12 registered voters living in the area). Because there is no requirement to show special benefit, Mello-Roos levies may be used to fund improvements of general benefit, such as fire and police facilities, libraries, and parks, as well as improvements that benefit specific properties. The provision also allows the reallocation of cost burdens to alleviate untenable burdens on specific properties.

Other Funding Sources

Other funding sources may be available to finance the various improvements associated with this Plan. Other sources include federal, state, regional, or local government grants, public agency construction, private developer coalitions, or various types of bonds not previously identified.

7.6. Maintenance Plan

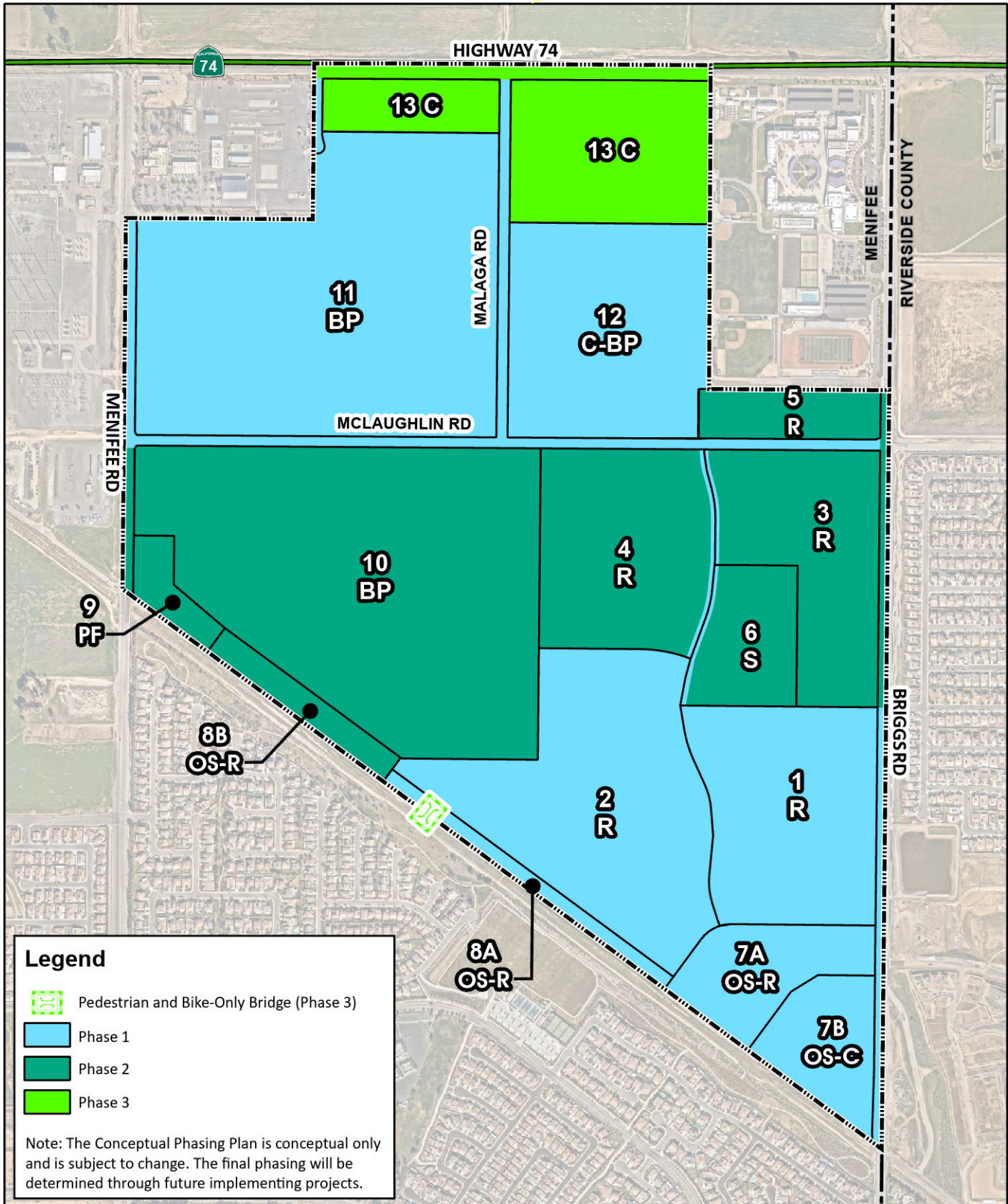
Public and private improvements constructed within the MVSP shall be maintained through a combination of public and private entities as described Table 7B, *Financing and Maintenance Plan*.

Table 7B provides a list of entity options that may fund and/or maintain facilities within the Specific Plan area; however, the ultimate maintenance entity for each facility will be determined by the MVSP's Development Agreement. A Homeowners Association (HOA) shall be established for the maintenance of common area landscape improvements and private roadways within the residential areas of the MVSP. Owners within the Commercial-Business Park and Business Park area shall be responsible for the maintenance of common area landscape improvements and private roadways within the commercial business park and business park areas of the MVSP. For areas in public ownership (such as public roadway ROWs), municipal maintenance districts may fund the maintenance of these areas.

Common areas identified in the Specific Plan shall be owned and maintained as follows:

- A permanent master maintenance organization shall be established for the Specific Plan area, to assume ownership and maintenance responsibility for all common recreation, open space, circulation systems, and landscaped areas. Separate maintenance organizations may or may not be established for the Business Park and Commercial-Business Park areas separate and apart from the rest of the Specific Plan area including the Residential planning areas. The organization(s) may be public or private and more than one entity and/or sub-associations/cost centers involved. A merger with an area-wide or regional organization will satisfy this standard provided that such organization is legally and financially capable of assuming the responsibilities for ownership and maintenance. If the organization is a private association, then neighborhood associations may be established for each residential development, as needed, and such associations may assume ownership and maintenance responsibilities for neighborhood common areas.
- Unless otherwise provided for in these standards, common areas shall be conveyed to the maintenance organization as implementing development is approved or any subdivision is recorded.
- The maintenance organization shall be determined prior to or concurrent with recordation of any final subdivision map.
- All public roadways within the MVSP area will be designed and constructed to standards stated in this Specific Plan and will be entered into the City system of roads for operation and maintenance as approved by the City Council.
- Any private roads or accesses will be maintained by an association or other public/private entity.

- Roadway landscape within the right-of-way (such as the enhanced parkways), any hardscaping outside of any roadway right-of-way, shall be maintained by a public/private entity or other master association.
- All landscape within raised medians on public roadways shall be maintained by Caltrans for Highway 74 and the City of Menifee for Briggs Road, McLaughlin Road, Menifee Road, and internal public roadways.



Source(s): ESRI, RCTLMA (2023), Nearmap Imagery (2023)

Figure 7-1



Conceptual Phasing Plan

Table 7B - Financing and Maintenance Plan

SERVICE OR FACILITY	PARTY RESPONSIBLE FOR CONSTRUCTION	PARTY RESPONSIBLE FOR FINANCING	PARTY RESPONSIBLE FOR ONGOING MAINTENANCE AND REPAIRS
Highway 74			
<i>Pavement and Curbs</i>	Master Developer	TUMF + Developer Fee Credits or Master Developer	Caltrans
<i>Center Median</i>	Master Developer	TUMF + Developer Fee Credits or Master Developer	Caltrans
<i>Landscaping + Lighting</i>	Master Developer	TUMF + Developer Fee Credits or Master Developer	City CFD
<i>Sidewalk</i>	Master Developer	TUMF + Developer Fee Credits or Master Developer	City CFD
<i>Entry Signage</i>	Master Developer	Master Developer	Planning Area 13 Owner
Briggs Road, Meniffee Road, Malaga Road, McLaughlin Road, and Interior Public Roads			
<i>Pavement and Curbs</i>	Master Developer	Master Developer or City CFD	City
<i>Center Median</i>	Master Developer	Master Developer or City CFD	City CFD
<i>Landscaping + Lighting</i>	Master Developer	Master Developer or City CFD	City CFD
<i>Sidewalk</i>	Master Developer	Master Developer or City CFD	City CFD
<i>Trails outside of the Public ROW</i>	Master Developer	Master Developer	City CFD
<i>Entry Signage</i>	Master Developer	Master Developer	HOA or Business Park Owners
Private Roads and Drive Aisles			
<i>Pavement and Curbs</i>	Master Developer	Master Developer or Builder	HOA or Business Park Owners
<i>Landscaping + Lighting</i>	Master Developer	Master Developer or Builder	HOA or Business Park Owners
<i>Sidewalk</i>	Master Developer	Master Developer or Builder	HOA or Business Park Owners
Public and Private Parks and Recreation			
<i>Planning Areas 7A and 7B – Public Sports Park and Granite Hill</i>	Master Developer	Master Developer and City of Meniffee	City
<i>Planning Areas 8A and 8B – Greenbelts</i>	Master Developer	Master Developer	City CFD
<i>Pedestrian and Bike-Only Bridge over the Railroad Tracks</i>	Master Developer	Master Developer	City CFD
<i>Any Private Recreation Center(s) in Planning Areas 1-5</i>	Master Developer	Master Developer	HOA
<i>Any Agri-Commercial Areas in Planning Areas 5 and/or 13</i>	Master Developer or Builder	Master Developer or Builder	Builder/Operator
<i>Any Dog Park in Planning Areas 1-6</i>	Master Developer	Master Developer	HOA

SERVICE OR FACILITY	PARTY RESPONSIBLE FOR CONSTRUCTION	PARTY RESPONSIBLE FOR FINANCING	PARTY RESPONSIBLE FOR ONGOING MAINTENANCE AND REPAIRS
<i>Any Greenbelts in Planning Areas 1-6</i>	Master Developer	Master Developer	HOA
Public Facilities			
<i>Stormwater Drainage Facilities</i>	Master Developer	Master Developer	City CFD, RCFCWCD, or Business Park Owners
<i>Shared Detention/Water Quality Basins</i>	Master Developer	Master Developer	City CFD, HOA or Business Park Owners
<i>Private Lot Detention/Water Quality Basins</i>	Builder	Builder	Private Property Owner
<i>Sewer Facilities</i>	Master Developer	Master Developer	EMWD
<i>Water Facilities</i>	Master Developer	Master Developer	EMWD
<i>Planning Area 9 Public Facility - Civic Node</i>	City	City	City
<i>Any Public Elementary School in Planning Areas 1-6</i>	Romoland School District	Romoland School District	Romoland School District
Private Facilities			
<i>Residential Front Yard and Corner Side Yard Landscape - Single Family Lots</i>	Builder	Builder	Homeowner or HOA
<i>Residential Rear Yard Landscape - Single Family Lots</i>	Homeowner	Homeowner	Homeowner
<i>All Landscape - Multi-Family Residential</i>	Builder	Builder	Owner or HOA
<i>All Landscape - Business Park and Commercial-Business Park areas</i>	Builder	Builder	Owner or Tenant

Appendix A

General Plan

Consistency



Brookfield
Properties

A - General Plan Consistency Analysis

Applicable Policies	Menifee Valley Specific Plan Consistency Analysis
Land Use Element	
Goal LU 1: Land uses and building types that result in a community where residents at all stages of life, employers, and visitors have a diversity of options where they can live, work, shop, and recreate within Menifee.	
Policy LU 1.2: Provide a spectrum of housing types that match the jobs in the City and make it possible for people to live and work in Menifee and maintain a high quality of life.	Consistent: The proposed Project, through project design features (SP Section 4.6) and compliance with laws and/or mitigation measures, proposes a spectrum of housing types from Low Medium Density Residential to High Density Residential in close proximity to the proposed jobs producing land uses proposed on the Project site.
Policy LU 1.4: Preserve, protect, and enhance established rural, estate, and residential neighborhoods by providing sensitive and well-designed transitions between these neighborhoods and adjoining areas.	Consistent: The proposed Project, through project design features (SP Section 5.4.1) and compliance with laws and/or mitigation measures, proposes compatible transitions between land uses on site and adjacent neighborhoods.
Policy LU 1.7: Ensure neighborhood amenities and public facilities are distributed equally throughout the City.	Consistent: The proposed Project, through project design features (SP Section 3) and compliance with laws and/or mitigation measures, proposes neighborhood amenities and public facilities accessible to the site and surrounding areas which include parks, recreational facilities, trails, bike trails, and a Civic Node.
Policy LU 1.8: Ensure new development is carefully designed to avoid or incorporate natural features including washes, creeks, and hillsides.	Consistent: The proposed Project would preserve Granite Hill and supplement it with recreational trails. No development would occur in Planning Area 7B. (SP Section 2.2)
Policy LU 1.10: Buffer sensitive land uses from major air pollutant emission sources, including freeways, manufacturing, hazardous materials storage, and similar uses.	Consistent: The proposed Project, through project design features (SP Section 4 and 5) and compliance with laws and/or mitigation measures, sites certain land use areas that are located away from proposed sensitive land uses on site and in the vicinity of the Project site. The Project is not within proximity to an existing freeway (I-215) or any existing hazardous air polluting uses.
Policy LU 1.12: Implement the policies of the Housing Element that promote a range of housing options, types and affordable housing units, that will enable the City to achieve its share of the RHNA.	Consistent: The proposed Project provides housing at various densities (SP Section 2 and 4) which would contribute to the City meeting its RHNA obligations.
Goal LU 3: A full range of public utilities and related services that provide for the immediate and long-term needs of the community.	
Policy LU 3.1: Work with utility providers in the planning, designing, and siting of distribution and support facilities to comply with the standards of the General Plan and Development Code.	Consistent: The proposed Project, to the greatest extent possible through coordination with the City and all applicable utility providers, would implement project design features (SP Section 3) and comply

Applicable Policies	Menifee Valley Specific Plan Consistency Analysis
	with laws and/or mitigation measures pertaining to distribution and support of utility providers.
Policy LU 3.5: Facilitate the shared use of right-of-way, transmission corridors, and other appropriate measures to minimize the visual impact of utilities infrastructure throughout Menifee.	Consistent: The proposed Project, through project design features (SP Sections 4, 5, & 6) and compliance with laws and/or mitigation measures, would comply with all applicable aesthetic regulations meant to reduce visual impacts of utility infrastructures.
Goal LU 4: Ensure development is consistent with the Riverside County Airport Land Use Compatibility Plan.	
Policy LU 4.1: Ensure that land use decisions within the March Air Reserve Base areas of influence are consistent with applicable ALUCP. Comply with State law regarding projects subject to review by the Riverside County Airport Land Use Commission.	Consistent: The proposed Project, its project design features, regulatory compliance measures, and/or mitigation measures would be reviewed for consistency with all applicable MARB ALUCP policies prior to approval for construction. The Riverside County Airport Land Use Commission released an approval letter dated November 16, 2022, which includes four conditions of approval. Refer to Section 4.9.7.5 in Hazards and Hazardous Materials section of this Draft EIR.
Circulation Element	
Goal C 1: A roadway network that meets the circulation needs of all residents, employees, and visitors to the City of Menifee.	
Policy C 1.1: Require roadways to comply with federal, state, and local design and safety standards; meet the needs of multiple transportation modes and users; be compatible with the streetscape and surrounding land uses; and be maintained in accordance with best practices.	Consistent: The proposed Project, through project design features (SP Section 3.1 and Section 5) and compliance with laws and/or mitigation measures, would comply with all applicable roadway design regulations prior to approval for construction.
Policy C 1.2: Require development to mitigate its traffic impacts and achieve a peak hour Level of Service (LOS) D or better at intersections, except at constrained intersections at close proximity to the I-215 where LOS E may be permitted.	<p>Not Consistent: LOS was removed from CEQA as a significance threshold in 2019; CEQA now considers VMT the significance threshold for transportation impacts. Nevertheless, the project was evaluated against the City's General Plan for consistency regarding policies, including the LOS that the City strives to maintain. The proposed Project would be mostly consistent with the General Plan Street System. However, even with payment of the Project's fair share for recommended intersection improvements, existing or forecasted operational deficiencies cannot be fully improved to an acceptable LOS the City desires at several intersections because of right-of-way constraints.</p> <p>Refer to the Project Traffic Study in Appendix K-1 for an in-depth analysis of LOS impacts and improvements required (see Table 8.A) for consistency or inability to meet with the aspirational goals of this General Plan policy.</p>

Applicable Policies	Menifee Valley Specific Plan Consistency Analysis
Policy C 1.5: Minimize idling times and vehicle miles traveled to conserve resources, protect air quality, and limit greenhouse gas emissions.	Consistent: The proposed Project, through project design features (SP Section 4.7) and compliance with laws and/or mitigation measures (Refer to Section 4.3 Air Quality and Section 4.17 Transportation and Traffic), would comply with all applicable air quality regulations during construction and operation to the extent feasible. The Specific Plan sites commercial, recreational, industrial, school, and pedestrian and biking trails near on-site residential units, which would reduce vehicle miles traveled.
Goal C 2: A bikeway and community pedestrian network that facilitates and encourages nonmotorized travel throughout the City of Menifee.	
Policy C 2.1: Require on- and off- street pathways to comply with federal, state, and local design and safety standards; meet the needs of multiple types of users and meet ADA guidelines; be compatible with the streetscape and surrounding land uses; and be maintained in accordance with best practices.	Consistent: The proposed Project is consistent with the General Plan circulation system and, through project design features (SP Section 3.2 and Section 5) and compliance with laws and/or mitigation measures, would not significantly affect nonmotorized travel throughout the area. The proposed Project includes bikeway and pedestrian trails and sidewalks throughout the Project site. In addition, a pedestrian/bike trail would connect to an off-site bridge over the railroad line on the southern perimeter to the Project site connecting the proposed Project to the community of Heritage Lake.
Policy C 2.2: Provide off-street multipurpose trails and on-street bike lanes as our primary paths of citywide travel and explore the shared use of low-speed roadways for connectivity wherever it is safe to do so.	
Policy C 2.3: Require walkways that promote safe and convenient travel between residential areas, businesses, schools, parks, recreation areas, transit facilities, and other key destination points.	
Goal C 3: A public transit system that is a viable alternative to automobile travel and meets basic transportation needs of the transit dependent.	
Policy C 3.2: Require new development to provide transit facilities, such as bus shelters, transit bays, and turnouts, as necessary.	Consistent: The proposed Project includes a Civic Node that could site a potential passenger stop for trains using the rail line (SP Section 2.2). The Project as proposed does not include the development of bus turnouts; however, future development of the Specific Plan along Menifee Road would require consultation with the Riverside Transit Agency for potential bus turnouts and facilities (SP Section 3.1.4).
Open Space and Conservation Element	
Goal OSC 1: A comprehensive system of high-quality parks and recreation programs that meets the diverse needs of the community.	
Policy OSC 1.1: Provide parks and recreational programs to meet the varied needs of the community residents and make these facilities and services easily accessible and affordable to all users.	Consistent: The proposed Project would develop one public park, passive open space trails, and recreational amenities along the community's greenbelts and the potential elementary school site (SP Section 3 and 5).
Policy OSC 1.2: Require a minimum of five acres of public open space to be provided for every 1,000 city residents.	Consistent: The proposed Project would develop 29.8 acres of recreation and 14.7 acres of conservation space (SP Section 2.2) which exceeds

Applicable Policies	Menifee Valley Specific Plan Consistency Analysis
	the City's goal of five acres of public open space and parks per 1,000 city residents. Refer to Sections 4.15 Public Services and 4.20 Recreation of this EIR.
Policy OSC 1.3: Locate and distribute parks and recreational facilities throughout the community so that most residents are within walking distance of a public open space.	Consistent: The proposed Project would develop a public park and recreational facilities within walking distance of on-site residents. Refer to Sections 4.15 Public Services and 4.16 Recreation of this EIR.
Policy OSC 1.4: Enhance the natural environment and viewsheds through park design and site selection while preserving sensitive biological, cultural, and historic resources.	Consistent: The proposed Project would preserve Granite Hill as part of its conservation space area. The perimeter of the Specific Plan area would be designed per project design features (SP Section 5).
Goal OSC 2: A comprehensive network of hiking, biking, and equestrian recreation trails that do not negatively impact the natural environment or cultural resources.	
Policy OSC 2.1: Develop recreational trails for hiking, biking, and equestrian use throughout the city, making them, to the extent feasible, accessible to people of different neighborhoods, ages, and abilities.	Consistent: The proposed Project proposes recreational trails which would be accessible to the city to the extent feasible (SP Sections 3 and 4). There are no equestrian trails proposed on the Project site. Refer to Sections 3.0 Project Description and 4.16 Recreation of this EIR for discussion of proposed trails.
Policy OSC 2.2: Locate and regulate recreational trails so that they do not negatively impact the city's sensitive habitat, wildlife, natural landforms, and cultural resources.	Consistent: The proposed Project, through project design features (SP Section 3.2, 4.10, and 5) and compliance with laws and/or mitigation measures, would ensure that the trails would not impact wildlife habitat, natural landforms, and cultural resources.
Policy OSC 2.3: Recognize flood control facilities as potential locations for recreational trails and pursue these opportunities in coordination with the Riverside County Flood Control and Conservation District.	Consistent: The proposed Project proposes recreational trails within areas that have existing easements for Riverside County Flood Control storm drain facilities (SP Section 2.2).
Goal OSC 3: Undisturbed slopes, hillsides, rock outcroppings, and other natural landforms that enhance the City's environmental setting and rich cultural and historical past and present.	
Policy OSC 3.3: Encourage the use of clustered development and other site planning strategies to facilitate the preservation of the city's natural landforms, boulders, and rock outcroppings.	Consistent: The proposed Project, through project design features (SP Section 6) and compliance with laws and/or mitigation measures, would ensure Granite Hill is preserved as natural open space.
Goal OSC 4: Efficient and environmentally appropriate use and management of energy and mineral resources to ensure their availability for future generations.	
Policy OSC 4.1: Apply energy efficiency and conservation practices in land use, transportation demand management, and subdivision and building design.	Consistent: The proposed Project designs (SP Sections 5 and 6) would comply with all applicable energy conservation and alternative energy regulations prior to approval for construction and through operation. In addition, the Project site is not located in a State designated area for important mining resources (MRZ-2 Zone). Refer to Section 4.12 Mineral Resources of this EIR.
Goal OSC 5: Archaeological, historical, and cultural resources are protected and integrated into the city's-built environment.	

Applicable Policies	Menifee Valley Specific Plan Consistency Analysis
Policy OSC 5.1: Preserve and protect archaeological and historic resources and cultural sites, places, districts, structures, landforms, objects and native burial sites, traditional cultural landscapes and other features, consistent with state law and any laws, regulations, or policies which may be adopted by the city to implement this goal and associated policies.	Consistent: Development of the MVSP would comply with all application regulations regarding archaeological, historical, and cultural resources that may be affected by the proposed Project to ensure that no significant impacts would occur. For additional information, see Sections 4.5 Cultural Resources and 4.18 Tribal Resources of this EIR.
Policy OSC 5.3: Preserve sacred sites identified in consultation with the appropriate Native American tribes whose ancestral territories are within the city, such as Native American burial locations, by avoiding activities that would negatively impact the sites, while maintaining the confidentiality of the location and nature of the sacred site.	
Goal OSC 6: High value agricultural lands available for long-term agricultural production in limited areas of the City.	
Policy OSC 6.1: Protect both existing farms and sensitive uses around them as agricultural acres transition to more developed land uses.	Consistent: The City's General Plan and the Department of Conservation identify the Project site as Farmland of Local Importance, which is not a farmland category considered for the purposes of determining potentially significant impacts under CEQA. In addition, the existing Menifee Valley Ranch SP designation for the site signifies the City's intention of developing the MVSP area and not keeping the existing Farmland of Local Importance designation on the Project.
Goal OSC 7: A reliable and safe water supply that effectively meets current and future user demands.	
Policy OSC 7.1: Work with the Eastern Municipal Water District (EMWD) to ensure that adequate, high-quality potable water supplies and infrastructure are provided to all development in the community.	Consistent: The proposed Project, through project design features (SP Sections 2, 3, 7) and compliance with laws and/or mitigation measures, would comply with all applicable water usage regulations meant to reduce water demand, including use of wastewater or recycled water.
Policy OSC 7.2: Encourage water conservation as a means of preserving water resources.	Consistent: The proposed Project, through project design features (SP Sections 3 and 5) and compliance with laws and/or mitigation measures, would coordinate with EMWD and other applicable agencies to utilize water conservation techniques.
Policy OSC 7.3: Coordinate with the Eastern Municipal Water District to educate the public on the benefits of water conservation and promote strategies residents and businesses can employ to reduce their water usage.	Consistent: The proposed Project, through project design features (SP Sections 3 and 5) and compliance with laws and/or mitigation measures, would coordinate with EMWD for the utilization of reclaimed water for feasible applications on the Project site.
Policy OSC 7.4: Encourage the use of reclaimed water for the irrigation of parks, golf courses, public landscaped areas, and other feasible applications as service becomes available from the Eastern Municipal Water District.	Consistent: The proposed Project, through project design features (SP Section 3.3) and compliance with laws and/or mitigation measures, would coordinate with EMWD for the utilization of reclaimed water for feasible applications on the Project site including parks and public landscape areas.
Policy OSC 7.5: Utilize a wastewater collection, treatment, and disposal system that adequately serves the existing and long-term needs of the community.	Consistent: The proposed Project, through project design features (SP Section 3.4) and compliance with laws and/or mitigation measures, would utilize a

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	system that adequately serves the existing and long-term needs of the community.
Policy OSC 7.6: Work with the Eastern Municipal Water District to maintain adopted levels of service standards for sewer service systems.	Consistent: The proposed Project, through project design features (SP Sections 3 and 5) and compliance with laws and/or mitigation measures, would coordinate with EMWD to maintain adopted levels of service standards for sewer service systems. There are no septic tank systems proposed as part of the Project.
Policy OSC 7.7: Maintain and improve existing level of sewer service by improving infrastructure and repairing existing deficiencies.	Consistent: The proposed Project, through project design features (SP Section 3.4) and compliance with laws and/or mitigation measures, would maintain and improve existing level of sewer service by improving infrastructure and repairing existing deficiencies at adjacent connections, if necessary.
Policy OSC 7.8: Protect groundwater quality by decommissioning existing septic systems and establishing connections to sanitary sewer infrastructure.	Consistent: The proposed Project would connect to existing sewer lines owned and maintained by EMWD. The proposed Project would comply with all applicable regulations regarding sewage treatment systems during construction and through operation of the proposed Project. There are no septic tank systems proposed as part of the Project, nor are there existing septic systems on the Project site.
Policy OSC 7.9: Ensure that high quality potable water resources continue to be available by managing stormwater runoff, wellhead protection, and other sources of pollutants.	Consistent: The proposed Project, through project design features (SP Section 3.3 and 3.5) and compliance with laws and/or mitigation measures, would comply with all applicable storm water retention, discharge permits, and water supply regulations during construction and through operation of the proposed Project.
Policy OSC 7.10: Preserve natural floodplains, including Salt Creek, Ethanac Wash, Paloma Wash, and Warm Springs Creek, to facilitate water percolation, replenishment of the natural aquifer, proper drainage, and prevention of flood damage.	Consistent: The proposed Project, through project design features (SP Section 3.5, 5, and 7) and compliance with laws and/or mitigation measures, would facilitate water percolation, groundwater recharge, and flood prevention through cooperation with the City of Menifee, EMWD, the Riverside County Flood Control District, and other agencies during construction and through operation of the proposed Project.
Policy OSC 7.11: Ensure that natural and cultural resources are protected and avoided while still maintaining important water goals.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would protect and avoid natural and cultural resources while still maintaining important water goals. Refer to Section 4.4 Biological Resources, 4.5 Cultural Resources, and 4.18 Tribal Resources in this EIR.
Goal OSC 8: Protected biological resources, especially sensitive and special status wildlife species and their natural habitats.	

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Policy OSC 8.1: Work to implement the Western Riverside County Multiple Species Habitat Conservation Plan in coordination with the Regional Conservation Authority.	Consistent: The Project site was determined to be consistent with the applicable MSHCP and is not within a Criteria Cell, Cell Group, Core, or Linkage. Refer to Section 4.4 Biological Resources of this EIR and the project biology report in Appendix D-5.
Policy OSC 8.2: Support local and regional efforts to evaluate, acquire, and protect natural habitats for sensitive, threatened, and endangered species occurring in and around the city.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would support efforts to evaluate, acquire, and/or protect natural habitats for sensitive, threatened, and endangered species occurring in and around the city. Refer to Section 4.4 Biological Resources of this EIR.
Policy OSC 8.3: Partner with non-profit agencies at the local, regional, state, and federal level to fulfill the obligations of the MSHCP to preserve and protect significant biological resources.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would coordinate with agencies to fulfill the obligations of the MSHCP for potential impacts to biological resources. Refer to Section 4.4 Biological Resources of this EIR.
Policy OSC 8.5: Recognize the impacts new development will have on the city's natural resources and identify ways to reduce these impacts.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would recognize potential impacts on natural resources and would identify ways to reduce these impacts. Proposed avoidance, minimization, and/or mitigation measures are discussed in Section 4.4 Biological Resources of this EIR.
Policy OSC 8.7: Manage the recreational use of the city's unimproved open space areas for compatibility with sensitive biological resources as well as MSHCP Conservation Areas.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would preserve Granite Hill and establish limited passive recreational trails around the natural slopes (SP Section 3).
Goal OSC 9: Reduced impacts to air quality at the local level by minimizing pollution and particulate matter.	
Policy OSC 9.1: Meet state and federal clean air standards by minimizing particulate matter emissions from construction activities.	Consistent: The proposed Project would comply with all applicable air quality regulations during construction and operation and would mitigate impacts to the extent feasible. For additional information, see Section 4.3 Air Quality of this EIR.
Policy OSC 9.2: Buffer sensitive land uses, such as residences, schools, care facilities, and recreation areas from major air pollutant emission sources, including freeways, manufacturing, hazardous materials storage, wastewater treatment, and similar uses.	
Policy OSC 9.3: Comply with regional, state, and federal standards and programs for control of all airborne pollutants and noxious odors, regardless of source.	
Policy OSC 9.4: Support the Riverside County Regional Air Quality Task Force, the Southern California Association of Government's Regional Transportation Plan/Sustainable Communities Strategy, and the South Coast Air Quality	

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<p>Management District's Air Quality Management Plan to reduce air pollution at the regional level.</p> <p>Policy OSC 9.5: Comply with the mandatory requirements of Title 24 Part 1 of the California Building Standards Code (CALGreen) and Title 24 Part 6 Building and Energy Efficiency Standards.</p>	
Community Design Element	
Goal CD 1: A unified and attractive community identity that complements the character of the City's distinctive communities.	
<p>Policy CD 1.1: Enhance the city's identity through the use of distinct city graphics, such as the city seal, in the design of gateways, street signs, city signage, public facilities and public gathering spaces, and other areas where appropriate.</p> <p>Policy CD 1.2: Support the development and preservation of unique communities and rural and suburban neighborhoods in which each community exhibits a special sense of place and quality of design.</p> <p>Policy CD 1.3: Strengthen the identity of individual neighborhoods/communities with entry monuments, flags, street signs, and/or special tree streets, landscaping, and lighting.</p>	<p>Consistent: The proposed Project, through project design features (SP Sections 4 and 5) and compliance with laws and/or mitigation measures, would comply with all aesthetic regulations pertaining to the use of distinct city graphics for signage where appropriate, prior to approval for construction.</p>
Goal CD 3: Projects, developments, and public spaces that visually enhance the character of the community and are appropriately buffered from dissimilar land uses so that differences in type and intensity do not conflict.	
<p>Policy CD 3.1: Preserve positive characteristics and unique features of a site during the design and development of a new project; the relationship to scale and character of adjacent uses should be considered.</p>	<p>Consistent: The proposed Project, through project design features (SP Sections 4, 5, 6) and compliance with laws and/or mitigation measures, would comply with all aesthetic regulations prior to approval for construction. The Project is proposing to keep Granite Hill as protected open space, which is a unique geologic feature on the Project site.</p>
<p>Policy CD 3.2: Maintain and incorporate the city's natural amenities, including its hillsides, indigenous vegetation, and rock outcroppings, within proposed projects.</p>	<p>Consistent: The proposed Project, through project design features (SP Section 3, 4, 5) and compliance with laws and/or mitigation measures, would comply with all aesthetic regulations prior to approval for construction. The Project is proposing to keep Granite Hill as protected open space, which is a unique geologic feature (rock outcropping) on the Project site.</p>
<p>Policy CD 3.3: Minimize visual impacts of public and private facilities and support structures through sensitive site design and construction. This includes, but is not limited to appropriate placement of facilities; undergrounding, where possible; and aesthetic design (e.g., cell tower stealthing)</p>	<p>Consistent: The proposed Project, through project design features (SP Sections 4 and 6) and compliance with laws and/or mitigation measures, would comply with all aesthetic regulations prior to approval for construction. The Specific Plan contains a comprehensive discussion on design of buildings, parks, landscaping, and open spaces.</p>

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Policy CD 3.5: Design parking lots and structures to be functionally and visually integrated and connected; off-street parking lots should not dominate the street scene.	Consistent: The proposed Project, through project design features (SP Sections 3, 4, 5, 6) and compliance with laws and/or mitigation measures, would comply with all aesthetic and circulation regulations of parking prior to approval for construction. No off-site parking is proposed.
Policy CD 3.6: Locate site entries and storage bays to minimize conflicts with adjacent residential neighborhoods.	Consistent: The proposed Project, through project design features (SP Section 5 and 6) and compliance with laws and/or mitigation measures, would comply with all aesthetic regulations pertaining to site entries and storage bays prior to approval for construction.
Policy CD 3.8: Design retention/detention basins to be visually attractive and well-integrated with any associated project and with adjacent land uses.	Consistent: The proposed Project, through project design features (SP Section 5) and compliance with laws and/or mitigation measures, would comply with all aesthetic and stormwater regulations prior to approval for construction. The City would review basin design and functionality.
Policy CD 3.10: Employ design strategies and building materials that evoke a sense of quality and permanence.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would comply with all aesthetic regulations of the City prior to approval for construction, through the City's design review process.
Policy CD 3.12: Utilize differing but complementary forms of architectural styles and designs that incorporate representative characteristics of a given area.	Consistent: The proposed Project, through project design features (SP Section 6) and compliance with laws and/or mitigation measures, would comply with all aesthetic regulations prior to approval for construction, through the City's design review process.
Policy CD 3.13: Utilize architectural design features (e.g., windows, columns, offset roof planes, etc.) to vertically and horizontally articulate elevations in the front and rear of residential buildings.	Consistent: The proposed Project, through project design features (SP Section 6) and compliance with laws and/or mitigation measures, would comply with all aesthetic regulations prior to approval for construction, through the City's design review process.
Policy CD 3.14: Provide variations in color, texture, materials, articulation, and architectural treatments. Avoid long expanses of blank, monotonous walls or fences.	Consistent: The proposed Project, through project design features (SP Section 5 and 6) and compliance with laws and/or mitigation measures, would comply with all aesthetic regulations prior to approval for construction.
Policy CD 3.15: Require property owners to maintain structures and landscaping to high standards of design, health, and safety.	Consistent: The proposed Project design and Project maintenance plan (SP Section 7.6) would provide guidelines on maintaining structures and landscaping to high standards of design, health, and safety for the property owners on the Project site. The maintenance plan would be reviewed by the City prior to approval for construction.

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Policy CD 3.16: Avoid use of long, blank walls in industrial developments by breaking them up with vertical and horizontal articulation achieved through stamping, colors, materials, modulation, and landscaping.	Consistent: The proposed Project design would comply with all applicable aesthetics and landscaping regulations prior to approval of building permits for industrial buildings through its design review process.
Policy CD 3.18: Require setbacks and other design elements to buffer residential units to the extent possible from the impacts of abutting roadway, commercial, agricultural, and industrial uses.	Consistent: The proposed Project design (SP Sections 4, 5, 6) includes requiring setbacks and design elements to buffer residential units to the extent possible from the impacts of abutting roadway, commercial, agricultural, and industrial uses.
Policy CD 3.21: Use open space, greenways, recreational lands, and water courses as community separators.	Consistent: The proposed Project design, to the extent possible, would use open space, greenways, recreational areas, and water courses as community separators (SP Section 2, 4, 5).
Policy CD 3.22: Incorporate visual buffers, including landscaping, equipment and storage area screening, and roof treatments, on properties abutting either Interstate 215 or residentially designated property.	Consistent: The proposed Project design would comply with all applicable aesthetics requirements via the City's design review process prior to Project approvals. The Project is not located adjacent to I-215.
Goal CD 4: Recognize, preserve, and enhance the aesthetic value of the city's enhanced landscape corridors and scenic corridors.	
Policy CD 4.1: Recognize, preserve, and enhance the aesthetic value of the city's enhanced landscape corridors and scenic corridors.	Consistent: The proposed Project design (SP Sections 3, 4, 5, and 6) would comply with all applicable aesthetics and landscaping regulations pertaining to SR-74 (Eligible State Scenic Highway), Menifee Road (Eligible County Scenic Highway), and Briggs Road (Enhanced Landscape Corridor) prior to approval for construction and through operation.
Policy CD 4.2: Design new and, when necessary, retrofit existing streets to improve walkability, bicycling, and transit integration; strengthen connectivity; and enhance community identity through improvements to the public right-of-way such as sidewalks, street trees, parkways, curbs, street lighting, and street furniture.	Consistent: The proposed Project, through project design features (SP Sections 4, 5, 7) and compliance with laws and/or mitigation measures, would enhance community identity through improvements to the Project's vehicular and nonvehicular circulation plans. The designs would be subject to City design review prior to approval for construction.
Policy CD 4.4: Frame views along streets through the use of wide parkways and median landscaping.	Consistent: The proposed Project design (SP Sections 5 and 7) would comply with all applicable aesthetics and landscaping regulations pertaining to SR-74 (Eligible State Scenic Highway), Menifee Road (Eligible County Scenic Highway), and Briggs Road (Enhanced Landscape Corridor) prior to approval for construction and through operation.
Policy CD 4.7: Design new landscaping, structures, equipment, signs, or grading within the scenic corridors for compatibility with the surrounding scenic setting or environment.	Consistent: The proposed Project design (SP Sections 3, 4, 5, and 6) would comply with all applicable aesthetics and landscaping regulations pertaining to SR-74 (Eligible State Scenic Highway), Menifee Road (Eligible County Scenic Highway), and Briggs Road (Enhanced Landscape Corridor) prior to approval for construction and through operation.

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Policy CD 4.8: Preserve and enhance view corridors by undergrounding and/or screening new or relocated electric or communication distribution lines, which would be visible from the city's scenic highway corridors.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would comply with all applicable aesthetic regulations meant to reduce impacts to SR-74 (Eligible State Scenic Highway), Menifee Road (Eligible County Scenic Highway), and Briggs Road (Enhanced Landscape Corridor) prior to approval for construction.
Policy CD 4.9: Require specialized design review for development along scenic corridors, including but not limited to, building height restrictions, setback requirements, and site-orientation guidelines.	Consistent: The City would evaluate, through its design review process, the proposed developments adjacent to Menifee Road, Briggs Road, and SR-74 to ensure that building heights, setbacks, and building orientation is appropriate for the said corridors (SP Sections 4, 5, 6).
Goal CD 6: Attractive landscaping, lighting, and signage that conveys a positive image of the community.	
Policy CD 6.1: Recognize the importance of street trees in the aesthetic appeal of residential neighborhoods and require the planting of street trees throughout the city.	Consistent: The proposed Project design (SP Sections 4, 5, 6) incorporates the use of street trees in enhancing the aesthetic appeal of the residential areas of the Project. The Project would also plant street trees along the perimeter of the Project site per the applicable aesthetic and landscaping regulations regarding Menifee Road, Briggs Road, and SR-74. City review of the designs would occur prior to approval for construction.
Policy CD 6.2: Ensure that all public landscaping is adequately maintained.	Consistent: The proposed Project design and maintenance plan (SP Section 4, 5, 6, 7) would ensure that the responsible entity (or entities) for maintaining public landscaping on the Project site would be done to City approved standards.
Policy CD 6.4: Require property owners to maintain the existing landscape on developed nonresidential sites and replace unhealthy or dead landscaping.	Consistent: The proposed Project design and maintenance plan (SP Section 4, 5, 6, 7) would ensure that the responsible entity (or entities) for maintaining public landscaping on the Project site would be done to City approved standards.
Policy CD 6.5: Limit light leakage and spillage that may interfere with the operations of the Palomar Observatory.	Consistent: The proposed Project design (SP Section 5 and 6) would comply with all applicable light and glare regulations prior to approval for construction and throughout Project operation.
Policy CD 6.8: Discourage the use of flashing, moving, or audible signs.	Consistent: The proposed Project design (SP Section 5 and 6) would comply with all applicable signage lighting regulations prior to approval for construction and throughout Project operation.
Goal ED 1: A diverse and robust local economy capable of providing employment for all residents desiring to work in the city.	
Policy ED 1.2: Diversify the local economy and create a balance of employment opportunities across skill and education levels, wages and salaries, and industries and occupations.	Consistent: The proposed Project is consistent with the General Plan's stated goal of encouraging economic growth and job opportunities within the

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	city by providing employment opportunities on site through commercial, industrial, and civic land uses.
Policy ED 1.4: Provide sufficient infrastructure to serve the full build out of the city.	Consistent: The proposed Project is located in an area served by infrastructure, utilities, and public services and includes infrastructure improvements in the project design by widening roadways around the perimeter of the Project site and providing an elementary school site.
Goal ED 3: A mix of land uses that generates a fiscal balance to support and enhance the community's quality of life.	
Policy ED 3.3: Utilize the following parameters on general plan amendments that are not part of a city-initiated comprehensive amendment or update: <ul style="list-style-type: none"> Because retail uses provide retail sales taxes and lodging uses provide transient occupancy taxes, they provide the most lucrative fiscal balance. No general plan amendment changing from a land use designation that permits retail uses or lodging uses to a land use designation that does not allow retail or lodging uses shall be approved except in conjunction with a development agreement or other legally enforceable obligation on the property owner(s) that requires the subject property generate the same or better fiscal balance for the city as it would have generated with a retail or lodging use. Because office and industrial uses generate less demand for public facilities and services than residential uses, they provide a more lucrative fiscal balance. No general plan amendment changing from a land use designation that permits office or industrial uses to a designation that does not permit office or industrial land uses shall be approved except in conjunction with a development agreement or other legally enforceable obligation on the property owner(s) that requires the subject property generate the same or better fiscal balance for the city as it would have generated with an office or industrial use. <p>The city may require a fiscal impact analysis and mitigation of any negative fiscal impacts for any requested general plan amendment.</p>	Consistent: The proposed Project meets the following parameters through project design of adding commercial retail, office, and industrial uses to an existing SP area, through a Specific Plan and General Plan amendment on a site that is currently unplanned for commercial, industrial, or office uses. The proposed Project would add these uses in an area that has been planned for only residential and recreational uses.
Safety Element	
Goal S 1: A community that is minimally impacted by seismic shaking and earthquake-induced or other geologic hazards.	
Policy S 1.1: Require all new habitable buildings and structures to be designed and built to be seismically resistant in accordance with the most recent California Building Code adopted by the city.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would comply with all applicable regulations meant to design and build new habitable buildings and structures to be seismically resistant prior to approval for construction.

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Goal S 2: A community that has used engineering solutions to reduce or eliminate the potential for injury, loss of life, property damage, and economic and social disruption caused by geologic hazards such as slope instability; compressible, collapsible, expansive or corrosive soils; and subsidence due to groundwater withdrawal.	
Policy S 2.1: Require all new developments to mitigate the geologic hazards that have the potential to impact habitable structures and other improvements.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would comply with all applicable geologic hazard regulations meant to reduce impacts to habitable structures and improvements prior to approval for construction. Refer to Section 4.7 Geology and Soils of this EIR.
Policy S 2.2: Monitor the losses caused by geologic hazards to existing development and require studies to specifically address these issues, including the implementation of measures designed to mitigate these hazards, in all future developments in these areas.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would comply with all applicable geologic hazard regulations meant to reduce impacts to habitable structures and improvements prior to approval for construction. Refer to Section 4.7 Geology and Soils of this EIR.
Policy S 2.3: Minimize grading and modifications to the natural topography to prevent the potential for man-induced slope failures.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would comply with all applicable standards of approval for grading meant to prevent the potential for man-induced slope failures prior to approval for construction.
Policy S 2.4: Manage the groundwater resources in the area to prevent over drafting of the aquifers, which in turn could result in regional subsidence.	Consistent: The proposed Project would include piping for an on-site recycled water system and utilize it for irrigation if feasible. Through project design features and compliance with laws and/or mitigation measures, the proposed Project design would reduce potential groundwater extraction through drought tolerant landscaping and use of wastewater and recycled water where possible and would comply with all applicable water usage efficiency requirements and regulations meant to reduce water demand.
Goal S 3: A community that is minimally disrupted by flooding and inundation hazards.	
Policy S 3.1: Require that all new developments and redevelopments in areas susceptible to flooding (such as the 100-year floodplain and areas known to the City to flood during intense or prolonged rainfall events) incorporate mitigation measures designed to mitigate flood hazards.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would comply with all applicable flood safety regulations, including incorporation of mitigation measures designed to mitigate flood hazards. Refer to Section 4.10 Hydrology in this EIR.
Policy S 3.2: Reduce flood hazards in developed areas known to flood.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would not result in increased flood hazards to the existing area. Refer to Section 4.10 Hydrology in this EIR.

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Policy S 3.4: Develop floodplains as parks, nature trails, equestrian parks, golf courses, or other types of recreational facilities or joint-use facilities that can withstand periodic inundation wherever feasible.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, includes parks and recreation facilities that would withstand periodic inundation wherever feasible. Refer to Section 4.10 Hydrology in this EIR.
Goal S 4: A community that has effective fire mitigation and response measures in place, and as a result is minimally impacted by wildland and structure fires.	
Policy S 4.1: Require fire-resistant building construction materials, the use of vegetation control methods, and other construction and fire prevention features to reduce the hazard of wildland fire. Ensure all new development and/or redevelopment in the LRA and VHFHSZ will comply with the California Fire Code (CFC) and California Building Code (CBC). All new development within the LRA Very High Fire zone will comply with Chapter 49 of the California Fire Code and Chapter 7A of the California Building Code.	Consistent: The proposed Project is located within an LRA but not within a VHFHSZ. The proposed Project, through project design and compliance with laws and/or mitigation measures, would comply with all applicable fire safety standards as designed prior to approval. Refer to Section 4.20 Wildfire in this EIR.
Policy S 4.2: Ensure, to the maximum extent possible, that fire services, such as firefighting equipment and personnel, infrastructure, and response times, are adequate for all sections of the City. The City will continue to coordinate with the Riverside County Fire Department, for Interagency coordination, to respond to emergency calls in Menifee and to provide training and ongoing programs for public education.	Consistent: The City shall take a lead role to ensure public services are adequately provided at the Project site after construction and to the adjacent existing communities.
Policy S 4.4: Review development proposals for impacts to fire facilities and compatibility with fire areas or mitigate.	Consistent: The City shall take a lead role to ensure public services are adequately provided at the Project site.
Policy S 4.6: Coordinate with Eastern Municipal Water District to ensure adequate water availability for fire suppression.	Consistent: The City shall take a lead role to ensure there is adequate water availability for fire suppression on the Project site. The City has coordinated with EMWD by obtaining approval of the project specific WSA. In addition, the City's Fire Protection Plan conditions the project to ensure adequate availability of water for fire suppression.
Policy S 4.10: Ensure all new residential development as well as all new development and redevelopment within the LRA and VHFHSZ will comply with the most current version of the California Building Codes and California Fire Code.	Consistent: The proposed Project, through project design and compliance with laws and/or mitigation measures, would comply with all applicable fire safety standards as designed prior to approval.
Policy S 4.14: All new parcel maps and tentative maps in the LRA, SRA, and VHFHSZ shall provide two points of access to the project in conformance with the California Building Code and California Fire Code and CA GC 65302 (g)(5). Approval of parcel maps and tentative maps in LRAs, SRAs or VHFHSZs is conditional based on meeting the SRA Fire Safe Regulations and the Fire Hazard Reduction Around Buildings and Structures Regulations, particularly those regarding road standards for ingress, egress, and fire equipment access. (See Gov. Code, § 66474.02.).	Consistent: The proposed Project is in an LRA but not a VHFHSZ. Through project design and compliance with laws and/or mitigation measures, the Project would comply with all applicable fire safety and emergency response standards as designed prior to approval. Refer to Section 4.20 Wildfire in this EIR.

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Policy S 4.17: The City should ensure that all new development has adequate water, sewer, and fire protection consistent with the most current California Building Code and California Fire Code and will comply with the Board of Forestry and Fire Protection Fire Safe Regulations.	Consistent: The proposed Project would comply with all applicable regulations ensuring water supplies and pressures are available during a fire prior to approval for construction and throughout operation. Refer to Sections 4.15 Public Services and 4.20 Wildfire in this EIR.
Goal S 5: A community that has reduced the potential for hazardous materials contamination.	
Policy S 5.1: Locate facilities involved in the production, use, storage, transport, or disposal of hazardous materials away from land uses that may be adversely impacted by such activities and areas susceptible to impacts or damage from a natural disaster.	Consistent: The City would evaluate the proposed Project and Project site regarding facilities involved in the production, use, storage, transport, or disposal of potentially hazardous or toxic materials, prior to approval for construction. Refer to Section 4.9 Hazards and Hazardous Materials in this EIR.
Policy S 5.2: Ensure that the Fire Department can continue to respond safely and effectively to a hazardous materials incident in the city, whether it is a spill at a permitted facility, or the result of an accident along a section of the freeway or railroads that extend across the city.	Consistent: The proposed Project, through project design and compliance with laws and/or mitigation measures, would comply with all applicable fire safety and emergency response standards as designed prior to approval and throughout Project operation. Refer to Section 4.9 Hazards and Hazardous Materials in this EIR.
Policy S 5.4: Ensure that all facilities that handle hazardous materials comply with federal and state laws pertaining to the management of hazardous wastes and materials.	Consistent: The City would evaluate the proposed Project and Project site regarding facilities involved in the production, use, storage, transport, or disposal of potentially hazardous or toxic materials, prior to approval for construction and throughout Project operation. Refer to Section 4.9 Hazards and Hazardous Materials in this EIR.
Policy S 5.5: Require facilities that handle hazardous materials to implement mitigation measures that reduce the risks associated with hazardous material production, storage, and disposal.	Consistent: The City would evaluate the proposed Project and Project site regarding facilities involved in the production, use, storage, transport, or disposal of potentially hazardous or toxic materials, prior to approval for construction and throughout Project operation. Refer to Section 4.9 Hazards and Hazardous Materials in this EIR.
Policy S 5.6: Require all new industrial development projects and significant rehabilitation or expansion projects to reduce industrial truck idling by enforcing California's five (5) minute maximum law, requiring warehouse and distribution facilities to provide adequate on-site truck parking, and requiring refrigerated warehouses to provide generators for refrigerated trucks. Require air pollution point sources to be located at safe distances from sensitive sites such as homes and schools.	Consistent: The proposed Project would comply with all applicable air quality regulations during construction and operation and would mitigate impacts to the extent feasible. For additional information, see Section 4.3 Air Quality of this EIR.
Goal S 7: A community that has protected its sensitive structures, functions, and populations from the risks associated with climate change.	
Policy S 7.1: Continue to require environmental analysis for proposed projects which may produce harmful levels of greenhouse gas.	Consistent: The proposed Project would comply with all applicable air quality regulations during construction and operation and would utilize energy efficient equipment for heating and cooling and

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	facilitate use of alternative energy equipment and vehicles to the extent feasible. For additional information, see Section 4.3 Air Quality and Section 4.8 Greenhouse Gas Emissions of this EIR.
Policy S 7.3: Coordinate with energy providers to ensure reliable energy availability for the City's residents.	Consistent: The proposed Project design would comply with all applicable energy conservation and alternative energy regulations prior to approval for construction and through operation. For additional information, refer to Section 4.6 Energy of this EIR.
Policy S 7.9: Promote drought resistant landscaping to continue reducing water consumption and potential fuel sources.	Consistent: The proposed Project design ¹ would comply with all applicable landscaping and fire safety regulations prior to approval for construction and through operation.
Noise Element	
Goal N 1: Noise-sensitive land uses are protected from excessive noise and vibration exposure.	
Policy N 1.1: Assess the compatibility of proposed land uses with the noise environment when preparing, revising, or reviewing development project applications.	Consistent: A noise technical analysis was prepared for the proposed Project and is contained in Appendix J. The proposed Project, through project design features and compliance with laws and/or mitigation measures, would not be incompatible with the existing noise environment. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.
Policy N 1.2: Require new projects to comply with the noise standards of local, regional, and state building code regulations, including but not limited to the city's Municipal Code, Title 24 of the California Code of Regulations, the California Green Building Code, and subdivision and development codes.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, is subject to the noise standards of local, regional, and state building code, subdivision, and development code regulations. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.
Policy N 1.3: Require noise abatement measures to enforce compliance with any applicable regulatory mechanisms, including building codes and subdivision and zoning regulations, and ensure that the recommended mitigation measures are implemented.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would implement noise abatement measures that would comply with applicable regulatory mechanisms. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.
Policy N 1.6: Coordinate with the County of Riverside and adjacent jurisdictions to minimize noise impacts from adjacent land uses along the city's boundaries, especially its rural edges.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would protect surrounding land uses from excessive noise and vibration exposure, including adjacent County land outside of the city's boundaries. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.
Policy N 1.7: Mitigate exterior and interior noises to the levels listed in Table N-1 of the Noise Element to the extent feasible, for stationary sources adjacent to sensitive receptors.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would mitigate noise levels to the levels identified in the Noise Element to the

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	extent feasible; implementation of such measures would protect land uses from excessive noise and vibration exposure. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.
Policy N 1.8: Locate new development in areas where noise levels are appropriate for the proposed uses. Consider federal, state, and city noise standards and guidelines as a part of new development review.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, is located in an area where noise levels are appropriate for the proposed uses, which would protect surrounding land uses from excessive noise and vibration
exposure. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.	
Policy N 1.9: Limit the development of new noise-producing uses adjacent to noise-sensitive receptors and require that new noise-producing land be designed with adequate noise abatement measures.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would be subject to limitations governing development of new noise-producing uses adjacent to noise-sensitive receptors and would implement adequate noise abatement measures. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.
Policy N 1.10: Guide noise-tolerant land uses into areas irrevocably committed to land uses that are noise-producing, such as transportation corridors adjacent to the I-215 or within the projected noise contours of any adjacent airports.	Consistent: The proposed Project is located adjacent to SR-74 and within a projected noise contour of the March Air Reserve Base. This location would accommodate noise-tolerant land uses while minimizing impacts to adjacent land uses. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.
Policy N 1.11: Discourage the siting of noise-sensitive uses in areas in excess of 65 dBA CNEL without appropriate mitigation.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, does not include the siting of noise-sensitive uses in areas in excess of 65 dBA CNEL without appropriate mitigation. For additional information, refer to Section 4.13 Noise and Vibration of this IR.
Policy N 1.12: Minimize potential noise impacts associated with the development of mixed-use projects (vertical or horizontal mixed-use) where residential units are located above or adjacent to noise-generating uses.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would minimize potential noise impacts associated with mixed-use developments on site where residential uses may be located above or adjacent to noise-generating uses. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.
Policy N 1.13: Require new development to minimize vibration impacts to adjacent uses during demolition and construction.	Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would implement features and/or mitigation to minimize vibration impacts to adjacent uses during demolition and construction. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.

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<p>Policy N 1.14: Minimize vibration impacts on people and businesses near light and heavy rail lines or other sources of ground-borne vibration through the use of setbacks and/or structural design features that reduce vibration to levels at or below the guidelines of the Federal Transit Administration. Require new development within 100 feet of rail lines to demonstrate, prior to project approval, that vibration experienced by residents and vibration-sensitive uses would not exceed these guidelines.</p>	<p>Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would not be adversely impacted by vibration from the nearby rail line (not currently in use). Project development closest to the rail line would include design features that would reduce vibrations at or below the guidelines of the FTA. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.</p>
<p>Policy N 1.15: Employ noise mitigation practices and materials, as necessary, when designing future streets and highways, and when improvements occur along existing road segments. Mitigation measures should emphasize the establishment of natural buffers or setbacks between the arterial roadways and adjoining noise-sensitive areas.</p>	<p>Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would employ noise mitigation practices and materials, as necessary, when designing project streets and improvements along existing road segments. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.</p>
<p>Policy N 1.17: Prevent the construction of new noise-sensitive land uses within airport noise impact zones. New residential land uses within the 65 dB CNEL contours of any public-use or military airports, as defined by the Riverside County Airport Land Use Commission, shall be prohibited.</p>	<p>Consistent: The proposed Project is located beyond the 55 dB CNEL contour of March Air Reserve Base. The project was reviewed and approved by the Riverside County ALUC. For additional information, refer to Section 4.9.7.5 in Hazards and Hazardous Materials, and Section 4.13 Noise and Vibration of this EIR.</p>
<p>Policy N 1.18: Work with the Southern California Regional Rail Authority and railroad owners and operators to reduce the noise impacts on noise-sensitive uses adjacent to railroad tracks.</p>	<p>Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, and coordination with the Southern California Regional Rail Authority, would reduce noise impacts on noise-sensitive land uses that are adjacent to the railroad. For additional information, refer to Section 4.13 Noise and Vibration of this EIR.</p>
<p>Policy N 1.20: Adhere to any applicable Riverside County Airport Land Use Commission land use compatibility criteria, including density, intensity, and coverage standards.</p>	<p>Consistent: The proposed Project, through project design features and compliance with laws and/or mitigation measures, would adhere to ALUC's land use compatibility criteria, including density, intensity, and coverage standards. The project was reviewed and approved by the Riverside County ALUC. For additional information, refer to Section 4.9.7.5 in Hazards and Hazardous Materials, and Section 4.13 Noise and Vibration of this EIR.</p>

Source: City of Meniffee General Plan, 2013.

1 Section 5: Landscape Guidelines of the proposed Meniffee Valley Specific Plan encourages and identifies a drought tolerant plant palette. (Page 5-2)